

APPENDIX A

ROUTE ONE CORRIDOR, EXISTING CONDITIONS

REPORT



December 23, 2021



TOWN OF WESTERLY, RHODE ISLAND

Westerly Route 1 Corridor

Existing Conditions

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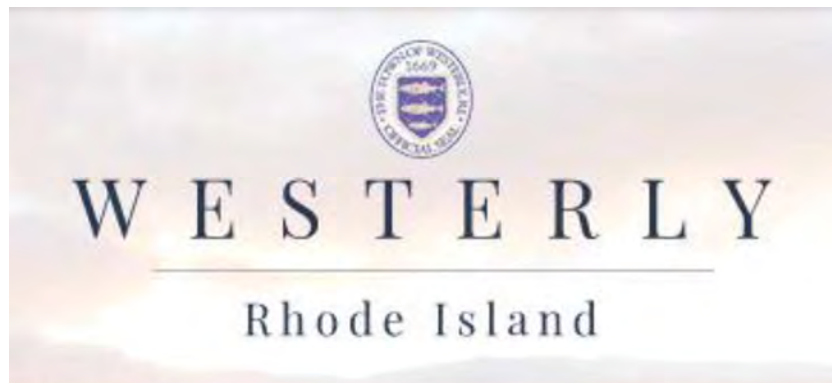
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1.0 INTRODUCTION

The Town of Westerly (the Town) is in the process of conducting a study of the Route One Corridor in order to identify strategies to encourage the type of development the Town and community would like to see along Route One to meet the goals of the 2020-2040 Comprehensive Community Plan. The final study will compile new analysis conducted as part of the project with historical work that has been done previously by various parties to develop a cohesive, implementable strategy for the Route One Corridor that will guide Town departments, the Planning Board, the Economic Development Commission, private properties owners and investors, and state agencies. The overall project consists of an existing conditions assessment, the implementation of a robust public engagement strategy, stakeholder engagement, and a final report providing a full analysis of site conditions and implementable strategies.

This particular report is intended to address the initial task of gathering and analyzing existing conditions within the Route One Corridor. Report development included gathering existing information from reports, studies, zoning information, and online mapping and data sets. This information was supplemented by a series of field visits. The information collected was compared against the goals and policies set forth in the Westerly Comprehensive Community Plan to ensure that future recommendations will align with on-the-ground conditions and intended outcomes. This report provides an overview of that process along with specific information related to the existing conditions data gathering that was conducted. Existing conditions data reviewed included issues related to zoning, traffic, pedestrian access, landscaping, site design, corridor circulation, parking utilization, access management, and the availability of infrastructure. All of these features contribute to the development and redevelopment of the area. This assessment has established the foundation for future tasks of the project which will include public engagement and the identification of implementation strategies.



2.0 EXISTING CONDITIONS

Understanding and assessing the existing conditions of an area are a critical first step and making recommendations on strategies for improvement and enhancement. Weston & Sampson conducted an in-depth review of existing conditions along the Route One Corridor as an initial step to provide the foundation for the next phase of the project which will outline strategies and actions the Town can consider achieving the overall goals for the area.

2.1 Project Area

The Route One corridor (herein the “Corridor”) in Westerly is an approximate five-mile stretch of roadway that connects Downtown Westerly to the Charlestown town line. It is the principal arterial roadway that provides access across the southern portion of Town, running east-west. This corridor includes the majority of the Town’s commercially developed land, predominantly occupied by large-scale developments in the form of shopping centers, strip malls, gas stations, and other relatively large retail establishments. The commercial corridor is flanked on its north and south sides by medium and high-density residential development. This area of Town developed incrementally over time without the benefit of an overarching plan to ensure the highest and best use of the land. The result is a sprawling linear strip of large-scale retail and service businesses, interspersed with single-family homes. These areas have little or no internal connectivity and are oriented to transportation by automobile with minimal opportunity for pedestrian accessibility. Likely due to the way it developed, area lacks an overall sense of character that is appropriate to the Town of Westerly.

2.2 Visual Analysis

The Route One Corridor Study began with a visual analysis of the corridor. This was conducted both online, through Google Earth and geographic information systems (GIS), taking a virtual tour of the corridor and understanding current land uses and development constraints, such as the presence of wetlands or the availability of infrastructure. This visual analysis was supplemented by corridor site visits. An organized site visit occurred on December 2, 2021, with members of the Route One Corridor Committee and members of the project team from Weston & Sampson and RKG. During this site visit attendees were provided with a table form to be used to take notes and record observations.

To date, two supplemental visits have been conducted by the Weston & Sampson project team members. During these visits a variety of conditions were observed. These observations ranged from specific issues such as traffic-related items, signage quality and placement, pedestrian access, site design, landscaping, vacancy rates, to overall corridor function and design. All of the site visit notes, and observations have been imported into the site visit observation form and are included in Appendix A. These site observations, both electronically and in person built the foundation for the corridor’s existing conditions. This baseline of information assisted in informing recommended strategies for implementation within this report, outlined in Section 9.0.

2.3 Zoning and Land Use

The Route One Corridor includes a variety of zoning categories along the Corridor, which also used throughout the Town. These categories vary between rural residential zoning and open space designations to highway commercial and industrially zoned land. Table 1.1 provides a summary of zoning designations within the study area, as well as the amount of land within each category. Zoning designation along the Corridor is shown in Maps ZO-1 through ZO-5 in Appendix B.

Table 2-1 Summary of Zoning Designations and Acreages within the Route One Corridor

Zoning Designation	Zoning Description	Approximate Acres with the Corridor
Residential Districts		
Rural Residential 60 (RR-60)	Intended for residential areas characterized by very low-density development and comprised mostly of land that is currently used for agriculture and contains wetlands, other severe soil constraints, or other sensitive environmental characteristics. The rural residential districts are designed to protect many of the Town's most valuable natural resources. Designed for those areas which do not have access to municipal sewers or municipal water.	185.0
Medium Density Residential 30 (MDR-30)	Generally intended for single-family neighborhoods adjacent to high-density areas, these zoning districts are designed to conform to existing development patterns rather than encourage major expansion beyond defined neighborhoods. Most areas within this district are connected to municipal water but are served by onsite wastewater treatment systems.	173.1
Medium Density Residential 20 (MDR-20)		92.3
High Density Residential 15 (HDR-15)	Intended primarily for areas of existing residential development. These zoning districts are designed for areas where existing densities are high, close to saturation, which include a variety of housing types. These areas are served by municipal water and municipal sewers.	2.5
High Density Residential 6 (HDR-6)		13.1
Commercial Districts		
Professional/Office (P-15)	Intended to establish areas within which the Town encourages a concentration of professional office and related uses. Property in this district often provides a transitional area between more intense districts and residential districts.	11.8
Neighborhood Business (NB)	Intended for areas characterized by small retail and personal service operations but surrounded by residential areas.	.2
Highway Commercial (HC)	Intended for areas which are primarily automobile oriented because of their location along major roads. An objective of this commercial zoning category is to address existing traffic safety problems associated with excessive curb cuts and to prevent future traffic problems from occurring with future development.	186.7
General Commercial (GC)	Intended for areas of historic commercial activity.	3.1
Shore Commercial – General (SC-G)	Intended to promote the use of waterfront locations for servicing local and tourist seasonal businesses and water-related activities.	14.6
Industrial Districts		
General Industry (GI)	Intended for manufacturing uses. This zone is intended for industrial uses which must be segregated because of their incapability with other land uses. It is designed to provide	250.5

	for infrastructure and operation requirements of industrial uses.	
Other Standard Districts		
Open Space and Recreation (OSR)	Intended for areas in use as open space and recreation. The district covers a variety of uses including the Town's well fields, major parks and recreation areas, portions of the barrier beaches and cemeteries.	93.9
Commercial Recreational (CR)	Intended for areas that have historically housed commercial/recreational facilities although adjacent to residential areas in order to allow their continued existence subject to reasonable regulations and limitations on future expansion.	281.3
Overlay Districts		
Airport Overlay	To regulate airport hazards in accordance with state mandate.	768.7
Aquifer Protection Overlay	To protect the Town's aquifers from contamination through incompatible land uses; to protect, preserve, and maintain the quality and quantity of the groundwater supply; and to protect the health, safety, and welfare of the public. The areas in need of protection include the public supply wells, groundwater reservoirs, and their associated recharge areas.	384.3
Bed and Breakfast Overlay	Create the alternative of short-term overnight accommodations in a residential setting for travelers and visitors to Westerly. It is recognized that bed and breakfast accommodations provide a valuable economic service to the Town in support of the tourism industry and help to preserve larger historic homes in the community by providing an opportunity for income support and continued use of the structure and maintenance of the property.	1332.0
Granite Street Overlay	Comprised of properties with frontage on Granite Street from the Grove Avenue intersection to its Tower Street intersection. Intended to permit additional professional offices and artistic studios to complement this area between downtown commercial and the more intense highway commercial.	13.9

Note: For the purposes of this summary table, only parcels that front on Route One were included in the total acreage count. If a parcel has frontage on Route One, the entire parcel was included in the acreage calculation.

The Town is also subject to an Airport Overlay District. The regulation of uses within proximity to an airport is enabled by RIGL §1-3-5(a). The APOD extends to much of the Town and is represented on Maps AO-1 through AO-5 in Appendix C within the Route One Corridor. For property close to the airport, the district does place height restrictions on structures to prevent safety issues with aircraft utilizing the airport facility. The district consists of the Runway Protection Zone, which restricts the height of uses and trees to 15 feet and limits uses to those related to surface parking, transportation uses, and plan and animal related agriculture. The district also identifies confliction areas and does not restrict uses within those areas but does require a vertical performance survey to be submitted to the zoning official for review.

Land use along the corridor is a mixture of small and large retail, service providers, strip mall developments, shopping plazas, hotels and motels, and residential development. The manner in which the land is used is relatively consistent with the zoning designations. There is an area just east of the intersection of Granite Street, Franklin Street, and East Avenue where several single-family homes are zoned as P-15, but for the most part parcels are zoned for their current uses. A conforming zoning designation is a benefit to the property owner because it avoids creating a non-conforming use where the use of the property is not allowed by the current zoning designation. Non-conforming zoning designations often require that a property owner seek permission from the zoning board to alter the property. Having a parcel that has zoning consistent with its use can streamline any changes in use, or changes in the structure. Maps LU-1 through LU-5 in Appendix D provides an overview of land use within the corridor.

The Town, through its Comprehensive Plan has identified the western end of the corridor as part of the Town’s urban services boundary, which identifies where development should be focused within the Town. The Town’s Future Land Use Map (map FLU-M1 within the comprehensive plan) also identifies the future potential expansion of the urban services boundary to encompass the eastern portion of the corridor all the way to the Charlestown town line (see Figure 2-1). Figure 2-1 shows the portion of Route One that is already within the urban services boundary (section within the red and white dashed outline) and the portion for future expansion of the urban services boundary (portion within the purple outline).

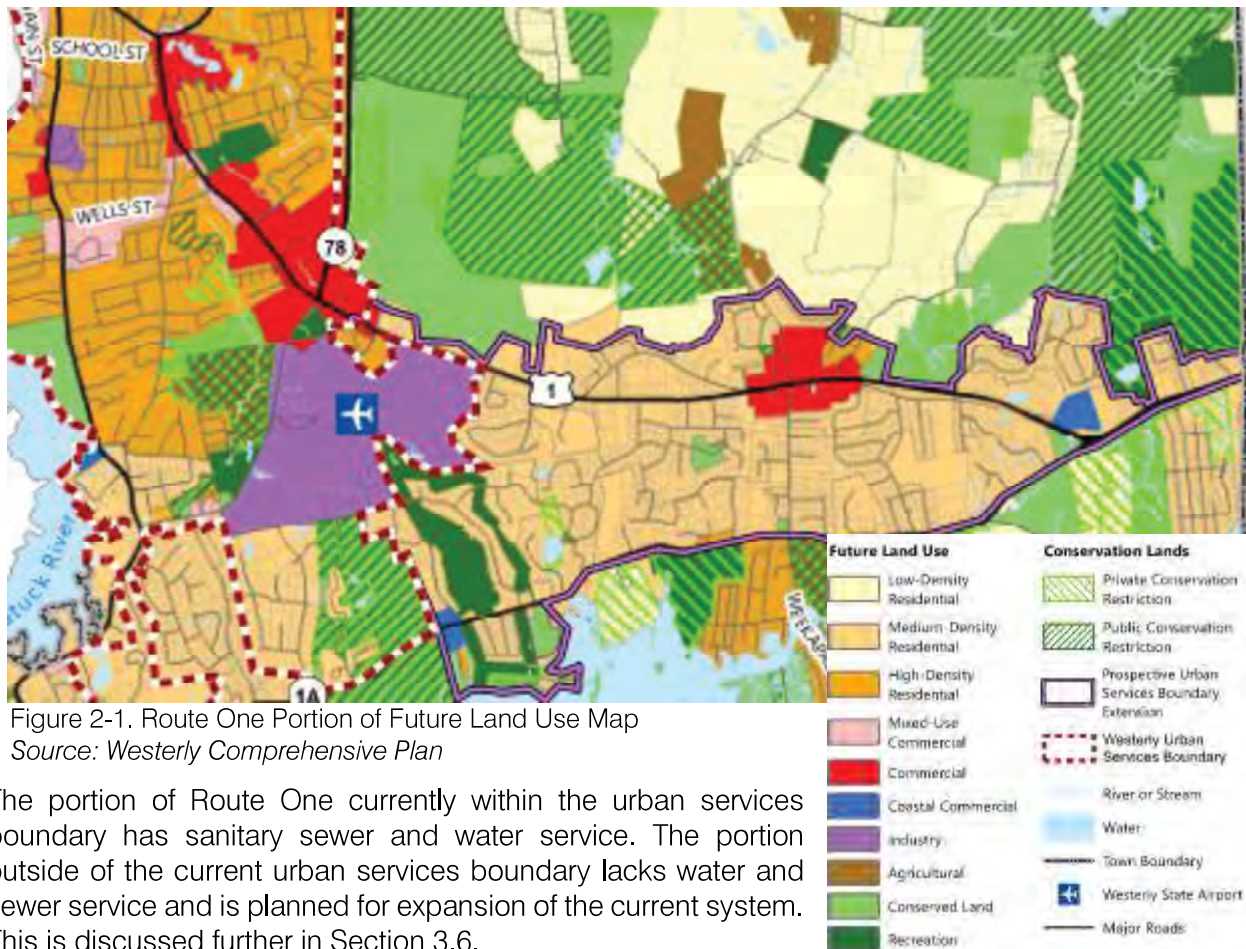


Figure 2-1. Route One Portion of Future Land Use Map
Source: Westerly Comprehensive Plan

The portion of Route One currently within the urban services boundary has sanitary sewer and water service. The portion outside of the current urban services boundary lacks water and sewer service and is planned for expansion of the current system. This is discussed further in Section 3.6.

3.0 CIRCULATION

The term circulation refers to the transportation mechanisms and systems (e.g., roads and sidewalks) that allow people to move around an area. For the Route One Corridor, this includes a variety of transportation options including automobiles, pedestrians, bicyclists, trucks, and transit and emergency vehicles. These different transportation modes use the circulation system, consisting of roadways, intersections, bike lanes, sidewalks, driveways, and parking areas. All of these components, when combined, contribute to circulation in and around the Route One Corridor. This system is important because it affects how people access the corridor itself, how they travel through it, and how they use it to access residential and commercial properties along it.

3.1 The Roadway

Route One is a state-owned road that runs in a generally north-south direction between the Massachusetts State Line on the north and the Connecticut State Line on the south. All maintenance and improvements within its right-of-way are the responsibility of the Rhode Island Department of Transportation (RIDOT). The roadway is classified by the by the Federal Highway Administration (FHWA) and RIDOT as a principal arterial road for the entire length it traverses through Westerly; therefore, its primary function should be to provide mobility, or the capacity to move traffic safely and efficiently and secondarily on accessibility to individual properties. The portion of Route One that is part of the study area is a four-lane road. The eastern segment is a divided highway with a center island that extends west for approximately one mile from the Charlestown town line. About one quarter of a mile west of the intersection with Shore Road, the center island ends and changes to a dual-use center turn lane. This dual-use center turn lane continues for approximately three miles, transitioning into an exclusive left-turn lane at the intersection with Route 78. The dual-use center turn lane in most of this area is approximately 20 feet wide, with the entire width of the roadway (from edge of pavement to edge of pavement) measuring approximately 100 feet. After the Route 78 intersection the center turn lane is no longer present and the remainder of the roadway consists of a four-lane cross section for approximately one mile until the Tower Street intersection at the terminus of the study area. The roadway infrastructure along Route One is shown on Maps INF-1 through INF-5 in Appendix E.

3.2 Traffic

Significant traffic congestion is a seasonal issue in Westerly. The summer influx of beachgoers and tourists increases traffic volumes and thus congestion issues in certain areas of the Town. The Route One Corridor is less affected by these seasonal fluctuations than other areas of Town because of its more inland location, although it does intersect with direct routes to the beach area. Route 78, however, is used as a the most direct connection between Interstate 95 and Route One. Day trippers looking to access the beaches may use certain sections of Route One, depending on their points of origination. Those visiting Westerly for longer stays are more likely to stay closer to the beaches and thus avoid the commercial areas of the corridor.

Misquamicut State Beach is Rhode Island's most popular state beach and is most directly accessed from Route 95 by taking Route 78, crossing Route One and continuing down Airport Road to Winnapaug Road allowing travelers to access the beach via Atlantic Avenue. This is the most heavily traveled route for beach traffic as a large number of Misquamicut Beach visitors are from Connecticut.¹ Alternatively,

¹ <https://riparks.com/history.php#misquamicut>

at the intersection of Route One and Langworthy Road (at Dunns Corners) travelers will head south on Langworthy Road from Route One to connect to Weekapaug Road connecting them with the beach on western end of Atlantic Avenue. These main beach access road intersecting with Route One can cause summer beach traffic congestion in these areas; therefore, the areas along Route One near the intersection of Dunns Corner Road and the intersection of Route 78, are most likely to see increased traffic due to summer tourist conditions. Although traffic is not currently at a level that causes significant traffic backups, it is an issue that must be considered when discussing attracting additional economic development, and thus potentially more traffic, to the Route One Corridor.

3.3 Traffic Signals and Signs

Along the five-mile stretch of Route One within the project limits, there are 14 signalized intersections. The locations of these signals are identified on Maps INF-1 through INF-5 in Appendix E. Two of the intersections are four-way intersections connecting Route One major streets including, Route 78 and Dunns Corner and Langworthy Roads. Several other signalized intersections primarily provide access to local residential areas (Warren Road, Wagner Road, Robin Hollow Lane, Sandy Hill Road, Wells Street, John Street, and Cross Street) the remaining signalized intersections provide controlled access point to commercial nodes or areas (Granite and Tower Streets, Granite, Franklin, and East Street intersection, Franklin Plaza, Mill Pond Plaza, and the Job Lot Plaza). Some of the signal equipment may not be compliant with current RIDOT timing and phasing standards as well as equipment requirements such as the use of backplates on signal heads to increase their visibility.

Existing signage along the corridor is dense and many times inconsistent. Signs in certain locations is confusing and overwhelming due to the placement, abundance, and nature of signs. Some of the directional (wayfinding) signs related to street names, emergency services, and others are obscured by retail signs. This makes navigation difficult for drivers who are trying to attend to traffic. In addition, some signs are faded and may not meet the Manual of Uniform Traffic Control Device (MUTCD) standards. In some locations (such as the western end of the corridor) multiple curb cuts in a short distance make effectively navigation to a destination difficult. This is further confused by the placement of signs that does not always clearly direct travelers to their intended location.

3.4 Parking

The design and location of parking areas along the corridor is different from location to location, but all occurs off the roadway on private property. In some locations there appears to be more parking than is needed to meet parking demand. This results in unnecessary hardscape that requires maintenance and supporting infrastructure (e.g., stormwater management systems). In other locations where the development is more compact (such as the western end of the corridor) parking appears to be inadequate due to small lot sizes and the repurposing of residential structures for commercial use. On-street parking is not permitted and should continue to be prohibited as on-street parking would be dangerous given Route One's functional classification as a principal artery as well as its narrowness, traffic volumes, and speeds. In some locations where development is denser, parking lot pavement from one site to another is continuous but you are unable to access one parking area to another, as asphalt berms separate the parking areas from each other. This causes travelers to have to exit on to the main road again to access the adjacent use and parking area. This increases the volume of traffic entering and exiting the roadway results in more potential conflicts between vehicles which ultimately increases congestion and reduces safety. It can also cause more potential conflicts with pedestrian and bicyclist along the corridor.

3.5 Pedestrian Access

Pedestrian access along the corridor is variable. This is due to intermittent sidewalks, which are inconsistent in width, accessibility and location. The eastern segment of the corridor from the Charlestown town line lacks any sidewalks and is divided roadway that is traveled at relatively high speeds. In fact, there are three signalized intersections in this area where signalized crosswalks are provided across Route One that do not connect to any existing sidewalks (Robbin Hollow Road, Wagner Road, and Warren Road). Sidewalks begin just east of the intersection with Dunns Corner Road at the South County Health Medical & Wellness Center on both sides of the road. They continue west for approximately one mile before the northern sidewalk ends at the intersection with Old Post Road. The sidewalk on the southern side of the road continues west for an additional 1,250 feet ending at the intersection with Sherwood Drive. Sidewalk on the northern side of the road begins again just west of Chamber Way. Sidewalk on the southern side of the road begins again just east of Bellaire Street and continues west for approximately 1.7 miles to the end of the corridor study area at Tower Street. The sidewalk on the north side ends at the intersection of the driveway to the Job Lot building and Wilder Ave and does not start again for approximately 1,700 feet west where it begins at the driveway for Pleasant Acres. From this point the northern sidewalk continues for the remainder of the corridor study area. See Maps INF-1 through INF-5 in Appendix E for a layout of sidewalks within the corridor.

This intermittent and disconnected extent of sidewalk makes pedestrian access along the corridor challenging if not impossible, especially for those with disabilities. Certain areas are accessible, but pedestrians cannot traverse the entire length of the corridor without crossing the street back and forth to access sidewalks, many times in areas with no formal crosswalks, or walk along the shoulder of the road. Both of these conditions are extremely dangerous to do in this stretch of roadway because of the high travel speed of vehicles and lack of street lighting. Many driveways to the various commercial locations along the corridor lack appropriate pedestrian crossings to aid those walking on the sidewalks.

3.6 Bicycle Access

The Route One corridor lacks defined bicycle accommodations such as bike lane, shared use paths, or on street shared use indications (signs or pavement markings) Route One is notoriously considered to be a dangerous route to navigate on a bicycle due to observed vehicle travel speeds, narrow travel lanes, and lack of paved shoulders in many areas.

3.7 Americans with Disabilities Act Requirements

Many of the existing sidewalks along the corridor do not meet current Americans with Disabilities Act (ADA) requirements. These include being too narrow as constructed or due to obstructions within the sidewalk (utility poles, signs, and mailboxes) that limit the available width. In addition, some of the accessible sidewalk ramps located at driveways and intersections are too narrow or too steep per the requirements and many lack the required tactile warning panels. Along the majority of the corridor the existing traffic signal lack the require Accessible Pedestrian Signal hardware including count down timers, audible signals, and vibrotactile push buttons.

4.0 ENVIRONMENTAL FEATURES

Parts of the Route One Corridor contain environmental features such as wetlands, aquifer recharge areas, terrain changes, and forested areas. These features contribute to the character of the corridor, while also presenting physical constraints to development. This section discusses both the aesthetic benefits and constraints to development associated with wetlands, soils and topography, and aquifer recharge areas.

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4.1 Wetlands

There are significant areas of wetlands adjacent to the corridor. Wetlands occur most prominently in the central areas of the corridor between Route 78 and Dunns Corner Road (see Maps EA-1 through EA-5 in Appendix F for all the wetland areas along the corridor). Wetlands are a constraint for development as there are required setbacks and prohibited activities associated with them. The corridor straddles Rhode Island Department of Environmental Management (RIDEM) and Rhode Island Coastal Resources Management Council (CRMC) jurisdictions, particularly on the east side of the corridor where Route One (i.e., Post Road) is the literal boundary between the two jurisdictional areas. (See Figure 4-1, right, “Freshwater Wetlands Jurisdictional Boundary: Westerly, R.I.”) This will potentially affect permitting approaches for projects that may arise from this study as, depending on project location, there may be a need to go to RIDEM or CRMC or both of the agencies for permitting decisions. Fortunately, RIDEM and CRMC generally attempt to keep their freshwater wetlands regulatory requirements congruent.

The State of Rhode Island is in the process of revising the jurisdictional areas that protect wetlands by limiting development activities. Changes are planned to go into effect January 15, 2022. Below we list the proposed general jurisdictional areas for wetlands in the Corridor as established by the State of Rhode Island:

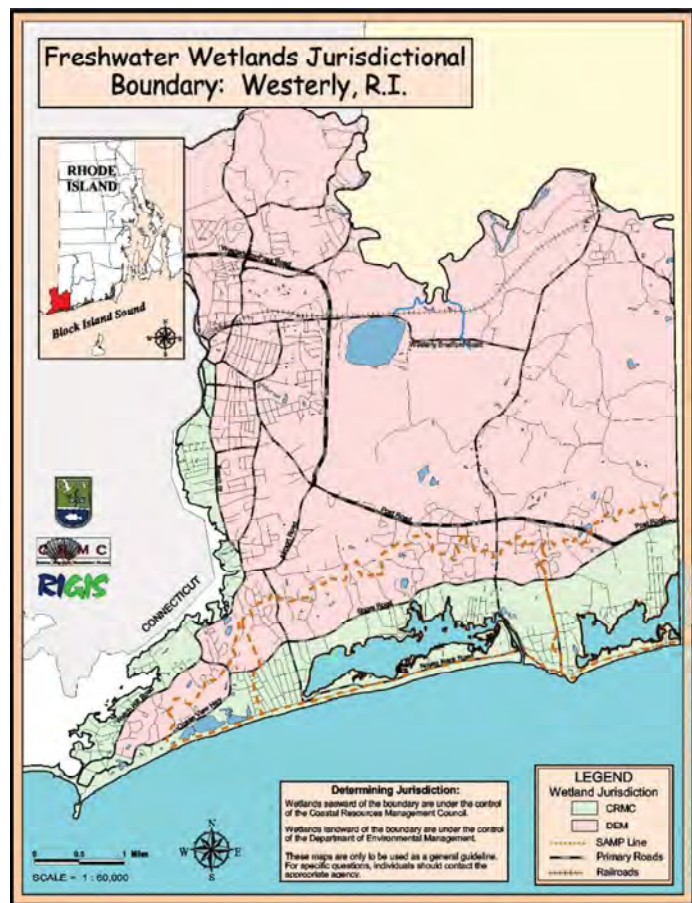


Figure 4-1. Freshwater Wetlands Jurisdictional Boundary
Source: RIDEM

- Up to 200 feet around any river or stream
- 50 – 100 feet around lakes and ponds.

As for RIDEM and CRMC jurisdiction, the Corridor straddles two regional jurisdiction for the proposed Freshwater Wetlands Regulations. Figure 4.2 depicts the jurisdictional regions. RIDEM's "Summary of Draft Revisions to the State Freshwater Wetlands Regulations" (November 2020) is provided in Appendix G to this Existing Conditions Report for reference.

4.2 Soils and Topography

The topography of the corridor has been significantly influenced by glaciation, creating a landscape that was shaped by glacial advancement and recession. The topography of the corridor is divided by the roadway itself. The land south of Route One is generally defined by coastal plain which is mostly glacial outwash, consisting of stratified layers of porous materials that washed away from the face of the glacier. With flat slopes and sandy loam, the coastal plain is easily developed, but sensitive to water quality impacts of development. The northern side of Route One is defined by a narrow band of intermittent slopes representing the terminal moraine, which is essentially the line where the glacier stopped its forward advance. This land is more difficult to develop because soils are relatively impermeable and steeper slopes are more prevalent. In the Rhode Island Stormwater Design and Installation Standards Manual (RIDEM and CRMC, 2015) (RISDISM), steep slopes, defined as 15% or greater, are restricted to development and should be avoided to the maximum extent practicable per Minimum Standard 1.

Soil types are also an important consideration when planning redevelopment activities. According to Table D-4 of RISDISM, Hydrologic Soil Group D, which primarily consists of areas with shallow bedrock as well as hydric soils and wetlands, should also be avoided. Maps of these and other environmental constraints are provided in Appendix F of this report.

4.3 Aquifer Recharge Areas

The Town has a designated Aquifer Protection Overlay District (APOD). The purpose of the APOD is to protect the Town's aquifers from contamination through incompatible land uses, to protect, preserve, and maintain the quality and quantity of the groundwater supply, and to protect the health, safety, and welfare of the public. The Town revised their existing APOD in 2021. These revisions were based on

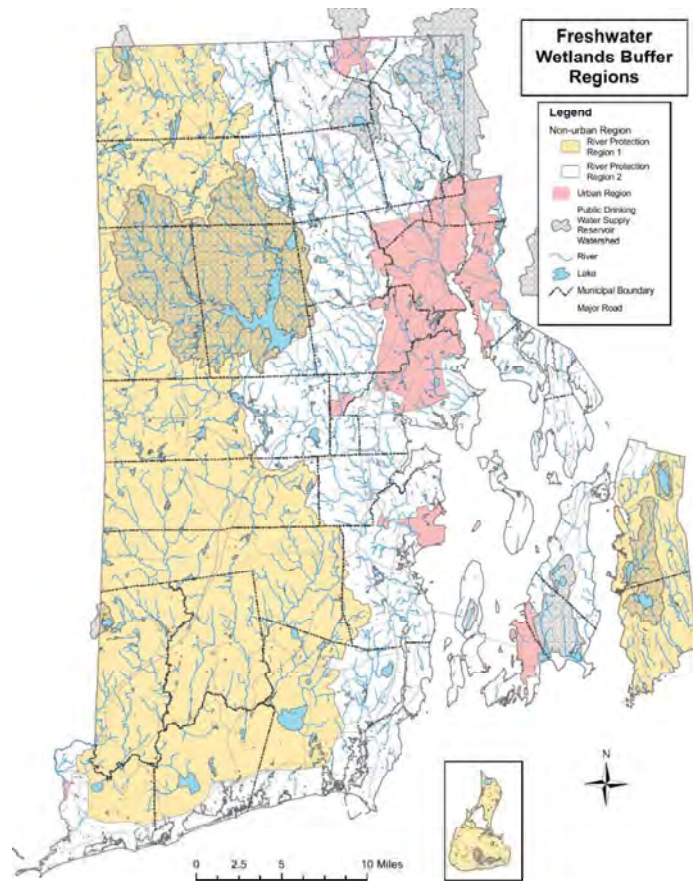


Figure 4-2. Freshwater Wetlands Buffer Regions

information and guidance from the Rhode Island Department of Environmental Management (RIDEM), after identifying deficiencies in the previous APOD related to the boundary of the district, the lack of guidance for permitting, and the allowance of higher risk uses without guidance designed to avoid contamination of groundwater resources.

The boundary of the new district (as shown in Figure 4-3) encompasses a significant portion of the Route One corridor between Route 78 and the Charlestown town line (the APOD is represented in Maps EA-1 through EA-5 in Appendix F as the community wellhead protection area). The specific revisions to the APOD were related to the boundary of the overlay district, the requirements for a permit and the implementation of best management practices, a restriction on the amount of cut and fill allowed, the prohibition of underground storage tanks, and the clarification of language including definitions for low impact development (LID). These changes were designed to better protect and regulate the resource, resulting in a higher design and development threshold within the district.

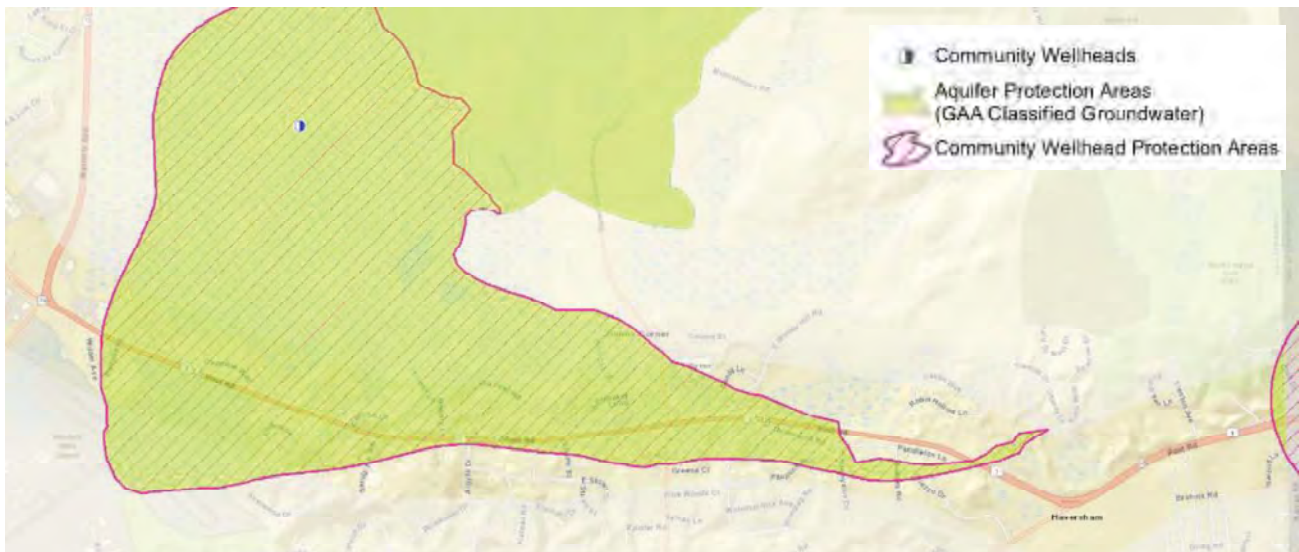


Figure 4-3. Aquifer Protection Overlay District in Route One Corridor
Source: Town of Westerly

5.0 WATER AND WASTEWATER INFRASTRUCTURE

Access to water and wastewater infrastructure is a make-or-break issue for many development and redevelopment options. As shown in Figure 5-2, the Town has defined an urban services boundary for development. As discussed in Section 3.3, the urban services boundary is directly related to accessibility to water and sanitary sewer. The urban services boundary is intended to encourage development in those areas of Town that have water and wastewater infrastructure. A portion of the corridor (west end) is within the existing urban services boundary and has existing water and sewer access. The east end is not currently within the urban services boundary but is proposed for future expansion of this area. In the expansion area there is water services, but no sanitary sewer service. Part of the planned expansion of the urban services boundary includes expansion of the sanitary sewer service.

5.1 Water Supply and Wastewater System

The Town has conducted a comprehensive well sites and distribution system provide sufficient capacity for present and future needs. Westerly's potable water system is funded through a self-sustaining enterprise program of the Town that is operated by the Department of Public Work's Utilities Division. The system serves the entire Route One corridor. The Town's drinking water is obtained from wells that tap the Bradford and Westerly groundwater reservoirs and pump from 12 supply wells at seven pumping stations. Water is stored in six storage facilities before being distributed for use through an extensive network of pipes totaling approximately 207 miles. The Town is continually monitoring, expanding, and upgrading the system to ensure high quality water supply to the residents and businesses in Town. The Town also owns and operates a wastewater collection and treatment system that serves approximately 45% of the Town. The Town operates one wastewater treatment plant that discharges treated effluent to the Pawtuxet River. The plant is currently designed to treat an average flow of 3.3 million gallons per day and a peak hourly flow of 7.8 million gallons per day. The plant is at 83.3% capacity and the Town is considering expansion plans for the plant. Figure 5-1 shows the water and sewer service availability along the corridor and Figure 5-2 shows the proposed expansion of sewer services within the urban services boundary.

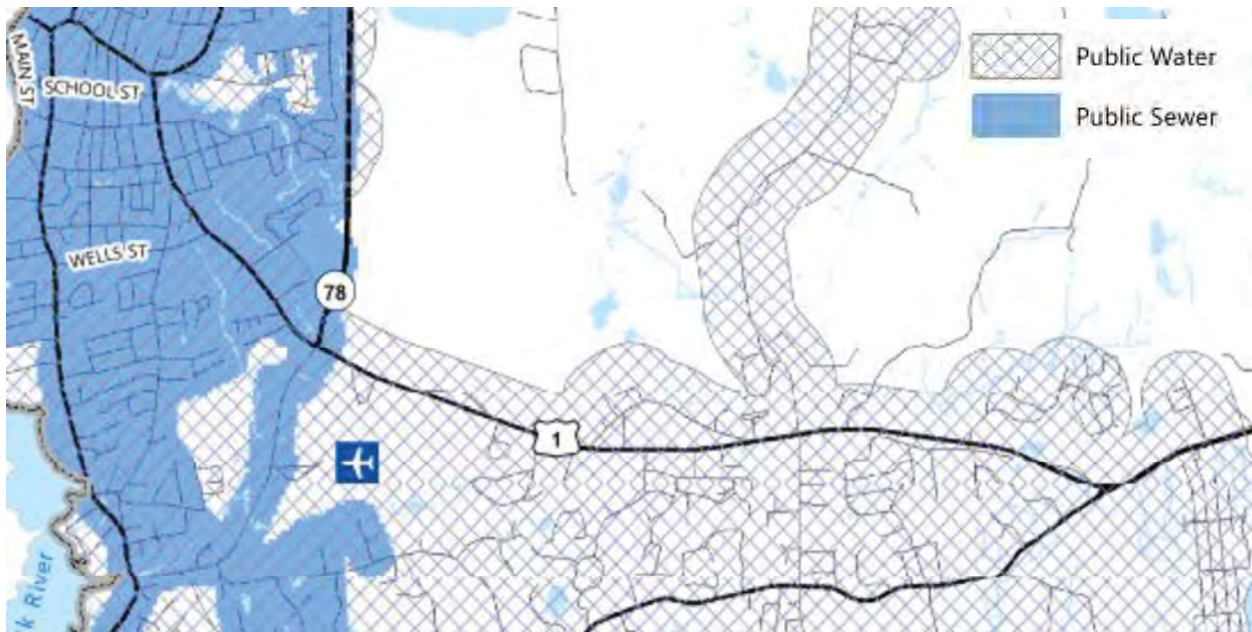


Figure 5-1. Water and Wastewater Service Route One Corridor
Source: Westerly Comprehensive Plan

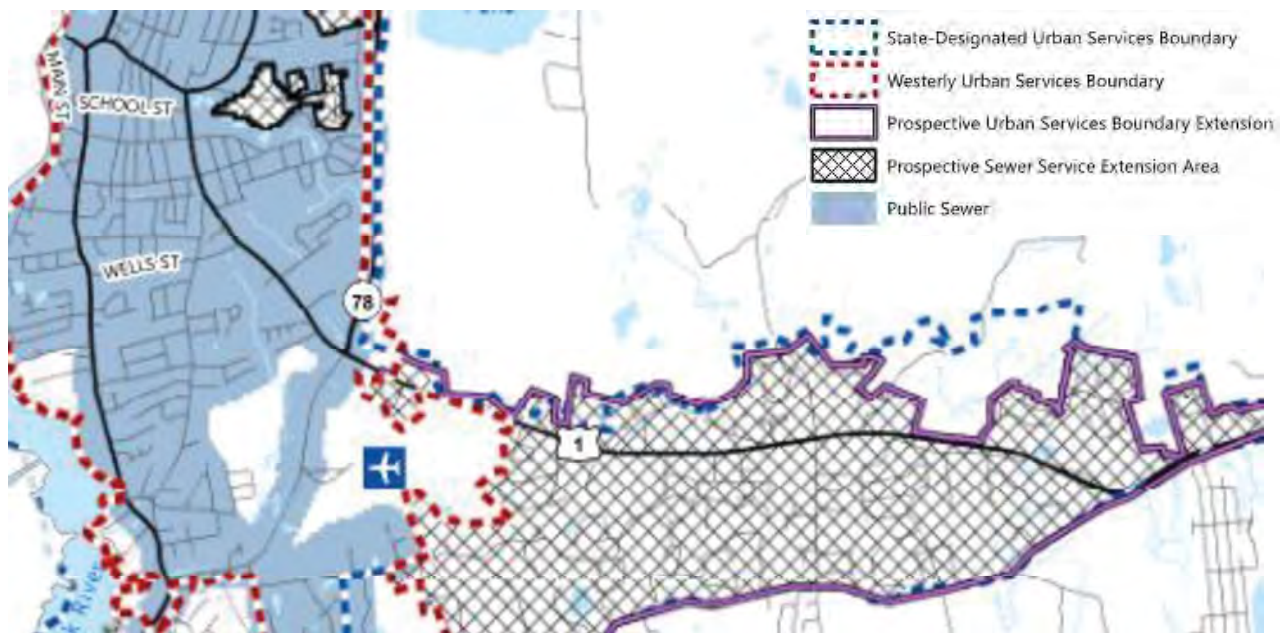


Figure 5-2. Water and Wastewater Service Route One Corridor
Source: Westerly Comprehensive Plan

6.0 LANDSCAPE CHARACTERISTICS

Landscaping along the corridor is minimal in many areas. There is relatively little landscaping in public areas and rights-of-way. Much of the existing landscaping was installed as part recent development and redevelopment. In other cases, property owners have installed landscaping on their own as part of personal efforts to beautify their property. This approach has created this inconsistent and sporadic landscaping pallet along the corridor.

In some locations (such as Franklin Plaza and Providence Coal Fired Pizza and adjacent lots) pavement abuts the sidewalk and roadway. There is little to no landscaped area between the pavement of the parking area of the commercial development and the roadway. The result is visually unappealing and Makes it difficult for drivers to spot entry ways and the boundary between the road and walkways.

Several of the strip mall developments have landscaped islands between their parking areas and the sidewalk and roadway but they are often sparsely planted and do little to create visual interest or curb appeal because they are not part of a larger landscaping effort and often get lost in the multiple curb cuts and overwhelming signage in place.

7.0 HOUSING

Housing appears in sporadic locations along the corridor, sometimes consisting of only a few homes in one area, to approximately a one and a half-mile stretch of roadway with predominantly single-family homes. The eastern end of the corridor is predominantly residential from the Charlestown town line to just east of the intersection with Dunns Corner Road where the development pattern transitions to commercial development in the form of big box retail and strip development. Heading west from Dunns Corner is another stretch of residential housing continues until the Ocean Chamber of Commerce and the airport. At the intersection of Routes 78 and One is another node of commercial development dominated by big box retail and strip development. The commercial development continues west dominating the landscape to the end of the corridor at the intersection with Tower Street.

The housing along the corridor is predominantly in the form of single-family housing. There are a few multifamily developments accessed from Route One along the corridor and several two-family homes. Many homes along the corridor have been converted to businesses and office spaces over time, leaving those currently being occupied as residences interspersed between commercial uses in some areas of the corridor. Maps LU-1 through LU-5 in Appendix D show the five segments of the corridor identifying parcels currently occupied as residential uses. Maps ZO-1 through ZO-5 in Appendix B show the zoning designation of parcels, which in some circumstances differs from the actual use of the property.

8.0 ECONOMIC CONDITIONS

8.1 Development History

The Route One corridor has followed a familiar pattern of economic activity through the course of its existence. Like many other state highways, in its approximate current form, the road was built in the early 20th century to facilitate automobile travel between major local population centers. The rural land surrounding the corridor became more valuable as Route One was constructed, leading to a continued pattern of automobile-oriented development starting in the beginning in the mid-20th century and continuing into the present. This development pattern focused on large and expansive parking facilities surrounding large retail buildings, though some multifamily housing borders the corridor and even single-family homes can still be found. Because of its role as a link between Westerly and points along the coast to the east, businesses on the corridor have tended to be destination-oriented and serve a regional draw. Fewer of the businesses today may be considered neighborhood retail establishments, for example, and chain stores predominate.

8.2 Current Market

Across the country, retailers have struggled as the COVID-19 pandemic continues to impact consumer behavior, accelerating existing trends in the retail market. Westerly and Route One are likewise experiencing those broader trends, with some uncertainty in the retail marketplace. The current market conditions could provide an opportunity to rethink sections of the corridor and its use composition. Some strip retail centers, and big-box retail sites appear to be struggling and vacancy on large outparcels is increasing; however, new retail centers (Westerly Crossings) have been built in recent years. That being said, the likely volatility of retail in the future suggests that a mix of uses would benefit the corridor by adding economic and employment diversity and thus stability. This form of development could help surrounding neighborhoods better connect to these amenities.

In addition to retail uses, there are some instances of small commercial office or medical office buildings throughout the Corridor, though these are intermittent. Because of Route One's somewhat limited interstate highway access, it is less likely that large national or regional office users will find it an appealing location, but the corridor could serve as a resource for smaller local businesses looking for office suites.

It is critical to note that the nature of Route One is distinct from Downtown Westerly, and in many ways accommodates uses that Westerly needs but may not be appropriate for, or encouraged to, locate in the traditional Town center.

8.3 Site Observations

As its history suggests, Route One is still built predominantly around automobile use and thus does not embody contemporary best practices in development. The single-use areas, imposing transportation infrastructure, inconsistent availability of sidewalks, prominent and expansive parking areas, and large and deep lots do not reflect Westerly's unique character, and may make it difficult to attract new tenants and residents. Recent development projects do not suggest that these patterns are likely to change without the Town's intervention, both in terms of infrastructure investment and zoning updates.

Currently, development is somewhat dispersed across the length of the corridor, making it difficult for visitors to visualize the identity or purpose of an individual location. There are nodes of activity—such as along Granite St, at Airport Road, and at Dunns Corners Road—but they are not easily distinguished from the general pattern of development along the corridor. Vacant and underused parcels are often located near these nodes.

9.0 SUMMARY OF FINDINGS RELATED TO EXISTING CONDITIONS

Key finding related to existing conditions are listed below:

- There are a variety of zoning categories throughout the corridor, representing a range of allowable uses; however, adjusting the existing categories to encourage specific uses would reverberate in other areas of Town that use the same zoning categories. An overlay district may be the simplest way to adjust zoning without adversely affecting other parts of Westerly.
- The roadway consists of varying lane and shoulder widths, which results in a range of vehicular travel speeds that can create confusion and frustration for people traveling through the Corridor, especially drivers.
- Traffic along the corridor is seasonal in nature and only congests certain intersections during summer months.
- Signs along the roadway are dense and inconsistent often leading to issues with wayfinding and access of specific properties.
- Parking areas in shopping plazas and strip malls appear to be sized for more parking than is in demand, often leaving large areas of parking underutilized.
- Some homes on smaller lots have been converted to businesses and now have inadequate parking.
- There is an overall lack of proper pedestrian amenities. Sidewalks along the corridor are intermittent and do not provide connectivity throughout. In locations where there are sidewalks many areas lack formal crosswalks and push button crossing signals. Utility poles, signs, and other obstacles have been placed within the sidewalks creating ADA compliance issues. The lack of these amenities, combined with relatively high travel speeds, make pedestrian access along the corridor dangerous.
- There are no bicycle accommodations along the Corridor. The narrow shoulder in some locations, coupled with high travel speeds, and the lack of shared-use paths or bike lanes present dangerous circumstances for bicycling in the Corridor.
- There are constraints to development in the form of environmental resource areas along the Corridor in various locations. Soil types, wetlands, and aquifer recharge areas will constrain development and redevelopment opportunities in some areas along the Corridor.
- The entire Corridor is served by public water but sanitary sewer available in only about half of the Corridor—the half of the Corridor west of Route 78. Notwithstanding, the Town has future expansion plans for sanitary sewer service in the eastern half of the Corridor.
- Landscape features along the corridor are typically minimal. There is relatively little landscaping in public areas and rights-of-way. The landscaping that does exist has been done piecemeal and is not consistent. The lack of landscaping is visually unappealing and, in some locations, presents difficulty with navigating parking areas and site entrances as landscape features can serve to identify the proper flow of travel in some circumstances.
- Housing appears in sporadic locations along the corridor, sometimes sporadically amongst commercial development, and also as a one-and-a-half-mile stretch, consisting of predominantly single-family homes.

- Economic conditions in the corridor are predominantly reliant on automobile use, which tends to detract from aesthetics and convenience for pedestrians and cyclists. Attracting tenants in this context may prove difficult. The current development is dispersed along the length of the Corridor with nodes of development at certain areas. These nodes are not easily distinguished from the general pattern of development because of the dispersed nature along the Corridor.
- There are nodes of activity—such as along Granite Street, at Airport Road, and at Dunns Corners Road—but they are not easily distinguished from the general pattern of development along the corridor. In other words, some of the most attractive development is essentially buried by less attractive surroundings.
- There are a number of relatively prominent vacant and underused parcels, which detracts from the aesthetics and marketability of the adjacent properties.

10.0 INITIAL IDENTIFICATION OF OPPORTUNITIES FOR ENHANCEMENT

In order to organize and assess the data that has been collected, several map sets have been created. These map sets display important information in a user friendly and easily accessible manner. Map sets included in this report are as follows:

- Appendix B: Zoning Map Set
- Appendix C: Airport Overlay Map Set
- Appendix D: Land Use Map Set
- Appendix E: Infrastructure Map Set
- Appendix F: Environmental Resource Areas Map Set
- Appendix G: Opportunities and Challenges Map Set

The entire Corridor is too long to display in one map and also present information at an appropriate resolution, so each map set is divided into five panels, with each panel representing a segment of the Route One Corridor. Each set of maps from Appendix A through Appendix F are existing conditions maps that provide a baseline of information with regard to what exists within the Corridor. Appendix G: Opportunities and Challenges Map Sets is a culmination of the information gathered from the other map series, as well as data collected from existing reports and studies, multiple site visit observations, and discussions with the Route One Corridor Commission. Using this information, areas of opportunity as well as challenges, have emerged through analysis and discussion.

The areas identified in Appendix G characterize general opportunities and general challenges that are representative of some specific locations, as well as general patterns observed throughout the Corridor. Certain types of opportunities and challenges repeat throughout the corridor. This report may only identify a few of these specifically but are intended to present concepts that will apply in multiple locations along the Corridor.

At this point, we identified opportunities and challenges in the public realm only. We have avoided identification of opportunities and challenges on private property because no private property owners have been notified. Identification of opportunities and challenges on private property will occur at a later phase of the project as these concepts are refined further through public input and stakeholder engagement.

Appendix G: Opportunities and Challenges Map Series has identified several categories. They are as follows:

- Access management areas
- Pedestrian access (sidewalks)
- Landscape improvement area
- Underutilized parking areas
- Intersections
- Strip malls/shopping plazas
- Low impact development areas
- Hotel/hotel

These categories are designed to encompass themes observed throughout the corridor that can be applied to multiple locations and areas.

11.0 NEXT STEPS

This is the first phase of the Route One Corridor project and focused on an evaluation of existing conditions. This evaluation was conducted in order to create a baseline of information in which general challenges for redevelopment and general opportunities for improvement could be identified. The next steps for this project will include:

- Identification of community goals, policies, and actions related to the Route One Corridor.
- A market analysis of the area focusing on type and quality of businesses and housing.
- The initiation of stakeholder and public engagement to gather public feedback and develop a vision for the Corridor. Public and stakeholder engagement will continue throughout the project.
- Identification of high priority areas or parcels for infill development or rehabilitation.
- Recommended zoning and regulatory updates needed to achieve community goals.
- Recommended strategies for implementation to achieve the overall goals and vision for the Corridor.

The next step tasks outlined above are not necessarily sequential in order, as some will overlap slightly, and some will occur simultaneously. This Existing Conditions Report and the next steps presented above will eventually culminate in a final report which will assist the Town in prioritizing strategies for development and redevelopment of the Route One Corridor.

APPENDIX A

Site Visit Observations



Identified Goal and/or Objective	Site Visit Observations
Traffic Related	
Reduce curb cuts	<ul style="list-style-type: none"> Excessive driveway access on individual lots and many appear to be larger than required for current use Potential for use of ring roads to connect multiple parcels to each other and side roads Issues with curb ramps being ADA compliant Specifically, area in the norther portion of the corridor (Chen’s, Dunkin Donuts, Gansett Wraps, Providence Coal Fired Pizza). Difficult to find your way in and out of locations and no connectivity between adjacent uses.
Traffic calming	<ul style="list-style-type: none"> Mix of signal types can create confusion and lack of awareness of signals
Sidewalk upgrades and maintenance	<ul style="list-style-type: none"> Sidewalk width appears to be narrow Appears to be issues with ADA compliance (insufficient width around utility poles and other structures) Sidewalk gaps prohibiting pedestrian connectivity
Excessive pavement and parking areas and parking lot design	<ul style="list-style-type: none"> Large pavement areas as a result of dual use center turn lane on eastern end of corridor (seems unnecessary due to lack of driveways and access requirements in this area) Large parking areas could be reduced, or additional structures placed there to infill Some drainage appears to need improvement which could aid in improving and maintaining long term pavement condition Several parking lots (Franklin Plaza) are not logically laid out and are confusing to navigate
Signage*	<ul style="list-style-type: none"> Some signs appear to not be MUTCD compliant (street names, directional, regulatory) Overabundance of signs (can cause driver overload and make wayfinding difficult) Appears to be some illegal signs in the ROW
Pedestrian access and bicycle accommodations*	<ul style="list-style-type: none"> Pedestrian signal equipment is not MUTCD and/or ADA compliant Many approaches at intersections are missing pedestrian signals all together



Identified Goal and/or Objective	Site Visit Observations
	<ul style="list-style-type: none"> • Several crosswalks go to nowhere at intersections (lack appropriate ADA landings) • No specific bicycle accommodations provided along the corridor • Lack of connections between commercial areas fronting on Route 1 and adjacent residential areas
Suggested Measures for Consideration	
<ul style="list-style-type: none"> • Retroreflective backplates on all signals to increase visual appearance and create uniformity. Shown to reduce crashes by 15% • Consider how to address lighting the corridor (is the target auto oriented or pedestrian oriented?) • Consider future sidewalk connections to residential areas beyond the commercial strip along Route 1 • Consider access management strategies: <ul style="list-style-type: none"> • Cut down the number of driveways per lot • Future cross access interconnection between properties • Create ring roads around the back of several properties that tie into side roads or signals • Consider shared use path on one side of roadway that accommodates bicycles and pedestrians • Incentivize shared parking, especially for small businesses that are adjacent to large parking areas and strip mall developments with excessive existing parking • Consider parking lot design requirements to ensure parking lots are logically laid out and easily navigable 	
Identified Goal and/or Objective	Site Visit Observations/Suggested Measures
Redevelopment of Underutilized Parcels	
Broaden uses in commercial districts	<ul style="list-style-type: none"> • Current HC zoning limits development potential • Overlay district could provide for a broader range of uses with additional incentives and design considerations • Desire for mixed use component in appropriate areas along the corridor • Single-use areas limits economic diversity, as do the large floorplates found throughout recent retail and office developments.



Identified Goal and/or Objective	Site Visit Observations
	<ul style="list-style-type: none"> • The length and diffusion of development along the corridor makes it more difficult to create a broader business ecosystem – harder for businesses and business owners to connect with each other and share resources. • It may have a greater positive impact to businesses if development and visitor activity is concentrated in specific nodes along the corridor rather than spreading development outward. Creating concentrated, more intensely developed nodes will provide visitors with multiple opportunities to shop, dine, and spend money in a single location. • Look for complementary commercial uses that will help diversify the business and use mix in the activity nodes, such as a variety of dining and drinking establishments, hotels, small office. Could also consider allowing small-scale boutique manufacturing where a business could produce and sell goods in the same space.
<p>Redevelop underutilized parcels</p>	<ul style="list-style-type: none"> • Vacant Staples lot has wetland limitations but good visibility from Route 78. Town has discussed the possibility of a hotel. Professional/medical office space was discussed during site visit. Issues related to sewer capacity with current service limited to a 4” pipe. A hotel use was also discussed here but could be limited by sewer infrastructure. Another question regarding access to this site was raised, is there an access easement or access agreement in place for site access through the existing parking lot of Job Lot. Could be potential mixed-use location with offices/retail below and residences above, if parking and sewer can accommodate. • Job Lot parking lot has excessive parking that could lend itself to additional commercial space. What are the limitations with ownership and/or lease agreements? A parking lot redesign could make site more welcoming and clarify roadways. Phragmites onsite could be addressed and the small waterway between the Staples site and Route 1 could be scenic. • Vacant lot next to Ocean Plaza and Midas has opportunities and is owned by Carpionato. They proposed a development that the Town was not receptive to. Could they be approached for discussion? South County Commons style redevelopment of the entire site with Ocean Plaza. • Vacant and underutilized parcel at Granite Center adjacent to quarry. Redevelopment into public park space with public water access and possibly restaurant or small shops in 2 existing buildings. Great opportunity for temporary pop-up installations of food trucks, semi-permanent food vendors, beer garden, farmers market, craft fair, etc. This could serve as a testing/proving ground for restaurant and food concepts, breweries, or other vendors looking to move from a pop-up to a more permanent brick and mortar location. Discussion with RIDEM regarding site



Identified Goal and/or Objective	Site Visit Observations
	<p>limitations due to quarry “wetland” designation. Are there any brownfield issues with the site that would unlock funding opportunities? Even if a site is suspected of contamination, funding may be available to confirm.</p> <ul style="list-style-type: none"> • Mill Pond Plaza has many empty storefronts and excessive parking/pavement area between the two buildings. Could this be redeveloped with second story residential to create a South County Commons style development? Will need to create enough financial incentive through zoning to encourage the current owner to either redevelop at a higher intensity and mix of uses or sell. These buildings may not be worth keeping given the size of the site. Mixed uses could blend with the residential neighborhoods that flank the site, and facilitate neighborhoods-scale businesses, like a coffee shop and café, with added greenspaces. Accommodating the Ace Hardware at the bank is a challenge (but perhaps not a big challenge).
<p>Focus development along the corridor for additional commercial space</p>	<ul style="list-style-type: none"> • Many existing parking areas would lend themselves to additional retail or commercial space within existing asphalt parking areas. Need to determine where lease agreements for parking spaces will hinder or prohibit this ability. Also need to determine if there is a market to support additional retail. Several existing retail-oriented plazas have vacancies suggesting there may be an over saturation of retail space in the corridor today. • Zoning district designation is varied throughout the corridor and some zoning designations or allowances could be changed. Zoning overlay allows more flexibility. • Focus zoning changes and development intensity where it makes the most sense and where opportunities for change are the greatest. Start small, be focused. Once success is realized, change will emanate from the core and permeate the rest of the corridor. • Couple public investment with private investment. Often, regulatory change is not the only catalyst for redevelopment. The Town may need to invest in public realm improvements and infrastructure to signal to property owners and the broader market that this is an area for investment and change.
<p>Suggested Measures for Consideration</p>	
<ul style="list-style-type: none"> • A mix of changes to existing zoning and the designation of a zoning overlay for the corridor are likely needed to remedy several of the determined issues. • Reduce or create flexibility in the parking regulations to reduce the amount of parking required to free up existing spaces for redevelopment. 	



Identified Goal and/or Objective	Site Visit Observations
	<ul style="list-style-type: none"> • Ensure zoning allows for more than one building to be built on a parcel of land. This could help encourage outlot development without having to subdivide land. • Identify proof-of-concept project sites where redevelopment is more straightforward as a means of encouraging developers to think more radically about other locations in the corridor. Successful proof-of-concept could then lead to more market interest by both developers and businesses, which could then lead to current owners redeveloping or repositioning assets or selling to developers who have the financial capability and knowledge to transform key properties. • Integration of multiple uses on individual parcels in addition to a district approach would create economic resiliency and improve efficiency of land usage. Leading with residential as the anchor to a major redevelopment/development can provide cash flow to the property if commercial/retail lease up is slow or vacancy is higher than expected. • Support development of smaller commercial spaces to create flexibility and support business mix/diversity. Smaller spaces for businesses also provide opportunities for local entrepreneurs to enter the market and create a more balanced mix of local and chain businesses.
Enhance Economic Activities	
<p>Support new initiatives for existing commercial and industrial areas to enhance business activity and increase job opportunities</p>	<ul style="list-style-type: none"> • Anecdotal evidence that existing complexity and inconsistency of application of zoning and development approvals hinders redevelopment. • Existing development patterns suggest that recent project proponents are unwilling or unable to take risks with form and use mix. • Look at broadening allowable uses in the zoning to encourage small scale manufacturing, co-working spaces, flex/R&D space, etc.
<p>Allow for mixed uses to attract new businesses and vitality to the area</p>	<ul style="list-style-type: none"> • Large parcels create challenges for a mixed-use district under existing zoning but provide opportunities for a mix of uses within a given parcel. • Airport is a dividing line: east is almost entirely large chain retail; west is smaller scale and more local/regional operators. This bifurcation offers different kinds of opportunities in each area – major redevelopment in the east, infill development to the west. • Vacancies seem to be highest in large outparcels in shopping centers or other large one-off retail spaces. This is a trend seen nationally as large standalone buildings and anchor stores struggle to compete, particularly in the face of COVID. • Ensure multi-family residential is allowed in parts of the corridor and dimensional regulations in the zoning are set to allow for larger-scale mixed use buildings.

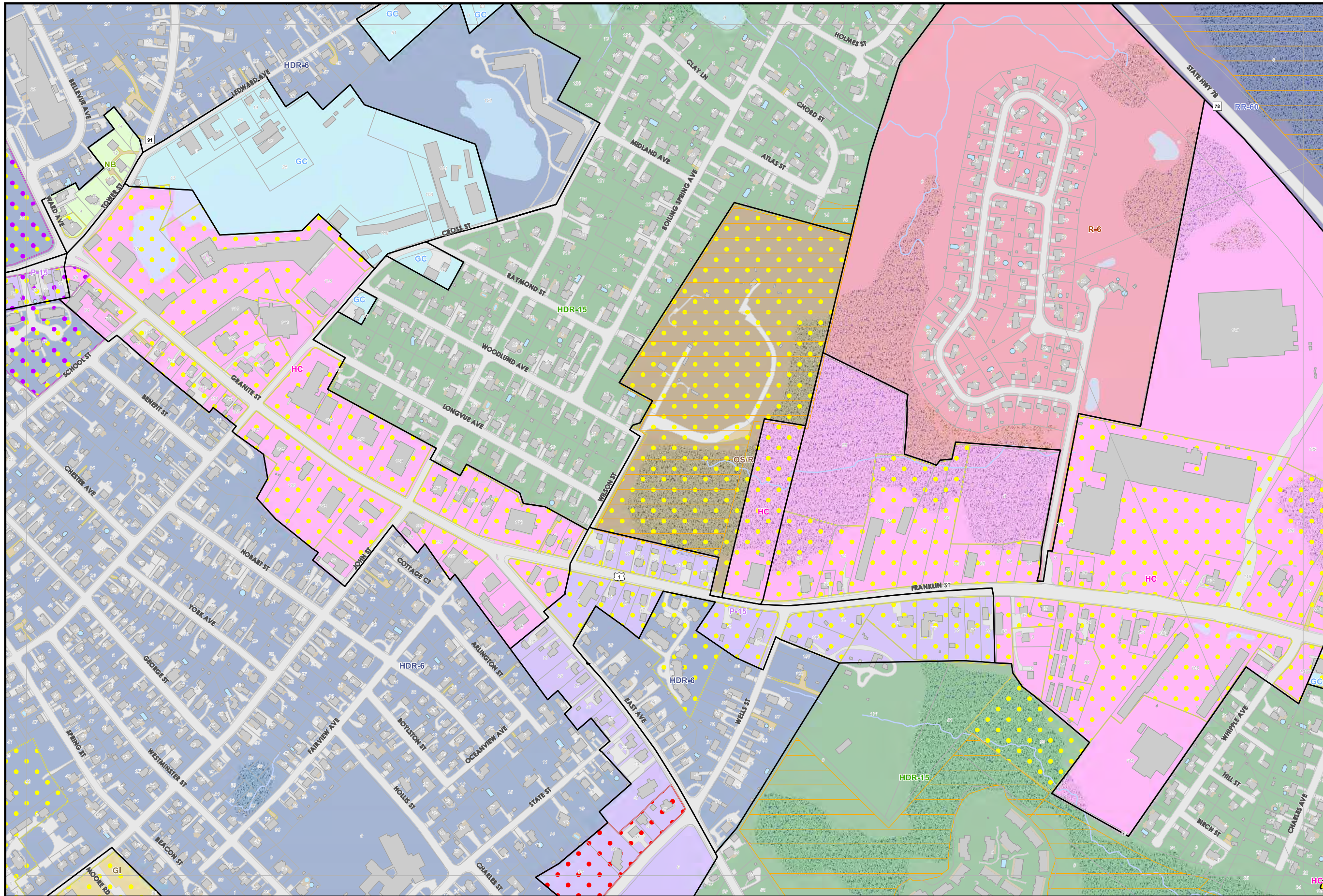


Identified Goal and/or Objective	Site Visit Observations
Suggested Measures for Consideration	
<ul style="list-style-type: none"> • Shared parking arrangements on larger parcels or across large retail centers and adjoining parcels • Simplified zoning and development approvals processes • Identify focus areas for additional density and provide clear guidelines, to create a bigger incentive for redevelopment and establish a clear vision • Allow mixed-use, multi-family residential where appropriate. 	
Aesthetic and Landscape Improvements	
Reverse the trend of generic commercial strip development	<ul style="list-style-type: none"> • Overabundance of strip mall style development lacking aesthetic features and amenities. Focused on strip development and large parking areas. • Individual lot development lacks connectivity to other adjacent parcels and adjacent strip malls. Adds significantly to traffic issues and signage confusion. • Bare walls facing corridor (such as at the Job Lot Plaza) could be addressed through landscaping or architectural treatment. Locate precedents where national corporate chains conform to local design standards.
Create a sense of place and reflection of small-town charm	<ul style="list-style-type: none"> • Auto oriented corridor detracts from shall-town charm and sense of place style development • Existing scale of the corridor – development and roadways – is entirely out of proportion with the rest of the town and may be more than is necessary in some places even with additional development. • Very little in the way of existing historic structures along the corridor that relate to Westerly’s broader identity. • Design principles for new development should be addressed with the ROCC and community. The ARB must have thoughts on this – lighting, signage, entryways, windows, etc.
Improve the streetscape with landscape improvements	<ul style="list-style-type: none"> • Parking lots extend to the street • Traditional stormwater draining structures abut roadways but don’t contribute to aesthetic features along the corridor



Identified Goal and/or Objective	Site Visit Observations
	<ul style="list-style-type: none"> • Landscape often serves to further separate sidewalk/street from building facades and entrances. • The corridor does have landscaping, but it appears unintentional. We need to define the roles landscaping will play here – to highlight entrances and paths, to create a cohesive aesthetic, to promote walkability, etc.
Suggested Measures for Consideration	
<ul style="list-style-type: none"> • Allow for infill of existing strip mall parking areas to increase commercial activity, break up the strip mall linear pattern and parking area, and enhance economic development within each parcel and throughout the corridor. • Require that roadside drainage features are dual purpose landscape features when possible • Require landscape buffers between the roadway and parking areas to provide a visual buffer and help in directing traffic flow within parking areas • Create set of detailed but flexible design guidelines to establish design principles for key locations throughout the corridor. Encourage new development to mimic scale of traditional neighborhoods in terms of block size, setbacks, etc. 	
Improved Land Use Pattern	
<p>Provide incentives for mixed-use commercial and professional services in redevelopment</p>	<ul style="list-style-type: none"> • Several plazas identified as opportunities for mixed use South County Commons style redevelopment (Mill Pond, Ocean Plaza) • Vacant Staples lot could support professional office space with residential development on the second floor • Granite Street section of corridor can be important link between downtown and Route 1 development – but it must complement, not supplant, existing downtown amenities and businesses. • Existing development is based on single-use district approach – not desirable enough to attract new or relocating businesses; nor unique – does not reflect Westerly’s character. • Single-use approach
<p>Create links between residential areas and Route 1 corridor</p>	<ul style="list-style-type: none"> • Residential areas that are adjacent to or within the corridor do not have linked pedestrian or bicycle access. Sidewalks are intermittent through the corridor to link residential sections with commercial sections and adjacent neighborhoods are not linked.

APPENDIX B
Zoning Map Sets



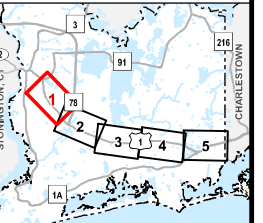
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Zoning and Overlay Areas

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westery Rhode Island.

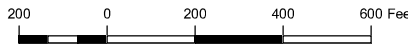
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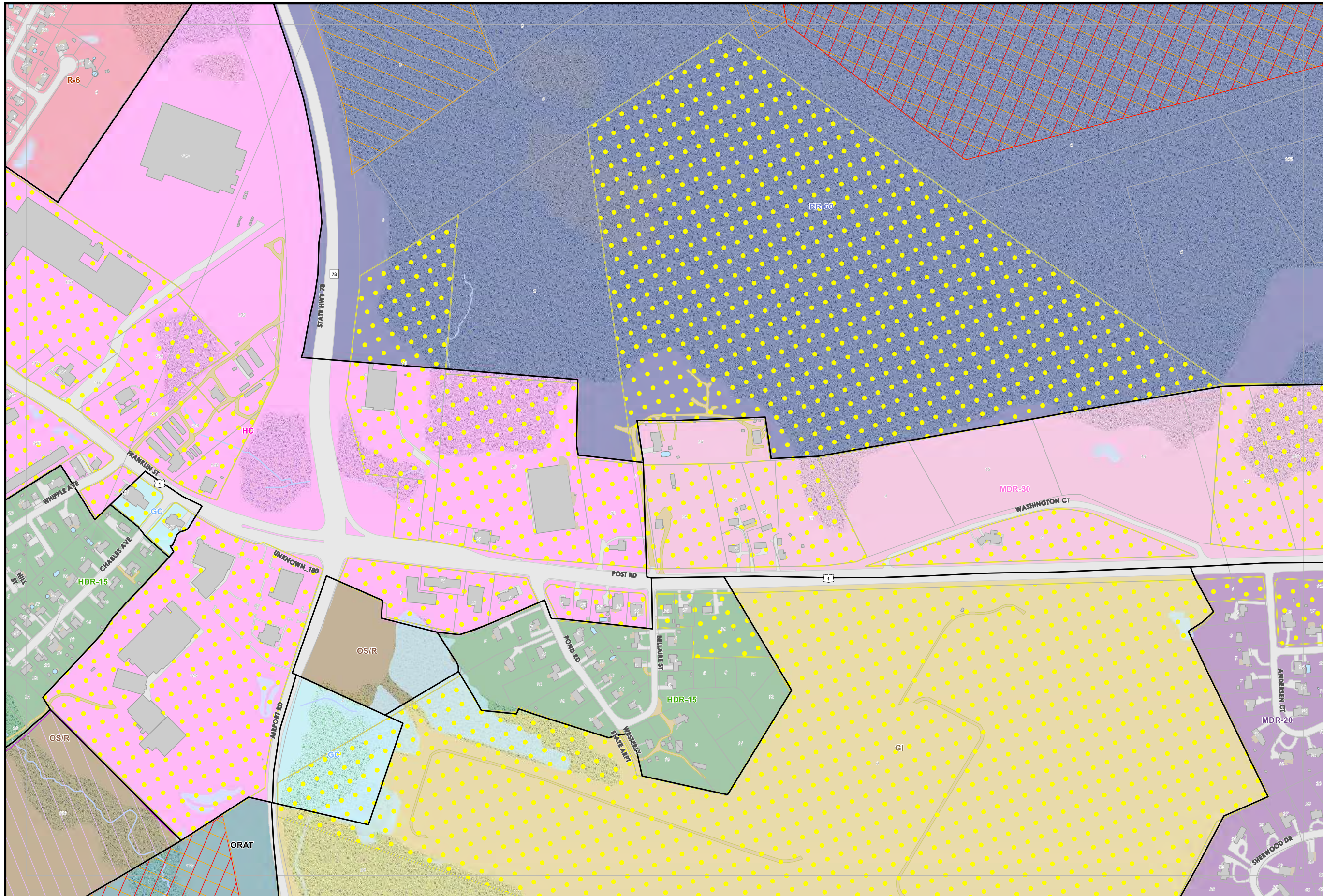
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- Parcels
 - State Conservation Areas
 - Local Conservation Areas
 - Overlaid "Conservation Intent"
 - Wilcox Park District
 - Down Town Historic District
 - Wells Street District
 - Granite Street District
 - Bed & Breakfast District
- Zoning**
- CR
 - DC-1
 - DCII
 - GC
 - GI
 - HC
 - HDR-15
 - HDR-6
 - LI
 - MDR-20
 - MDR-30
 - NB
 - ORAT
 - OSIR
 - P-15
 - PUD
 - R-6
 - RR-60
 - SC-G



Z0-1

MAP SHEET 1 of 5





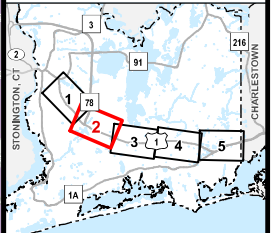
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Zoning and Overlay Areas

DECEMBER 2021

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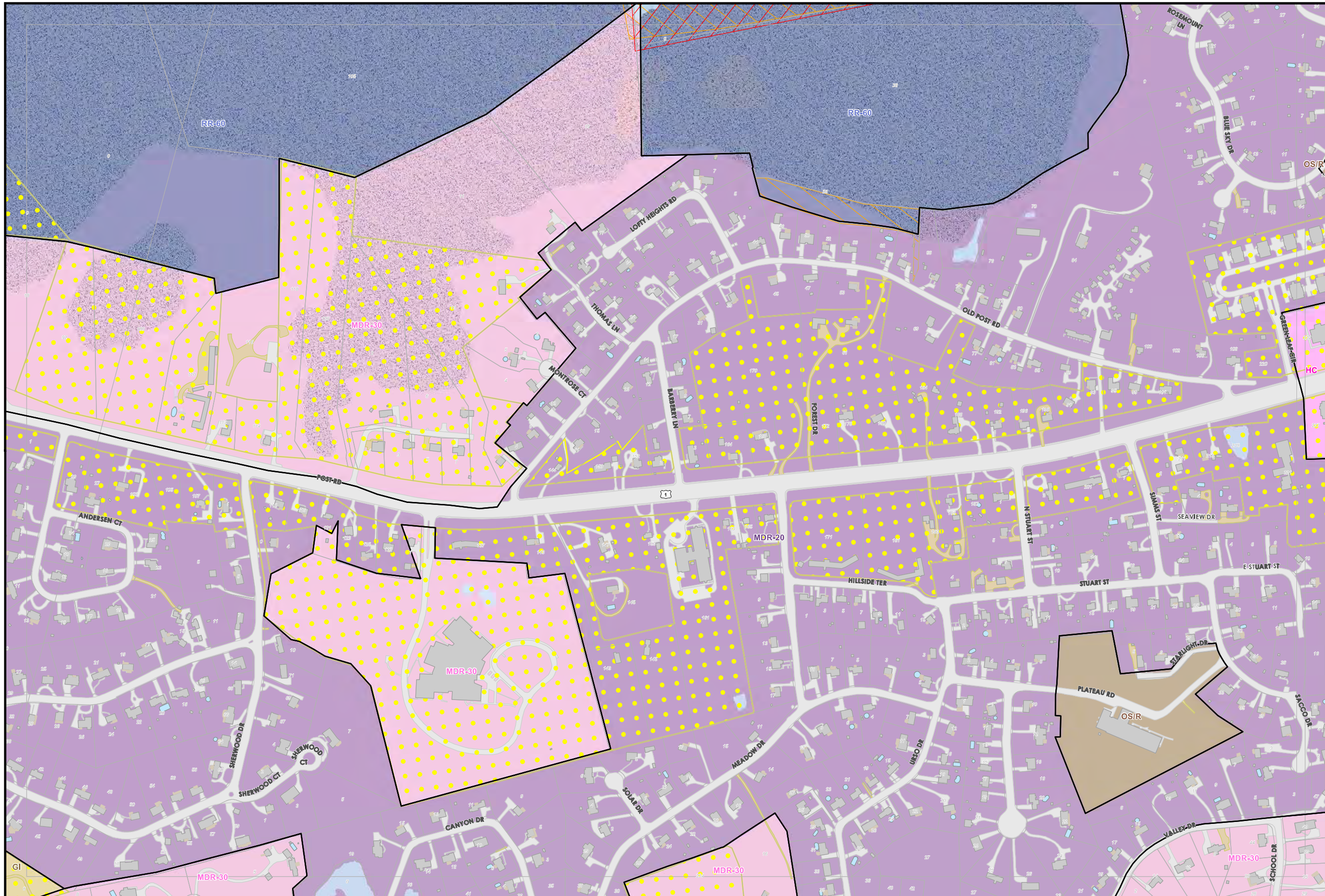
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 - Bed & Breakfast District
- Zoning**
- CR
 - DC-1
 - DC-II
 - GC
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 - HDR-15
 - HDR-6
 - LI
 - MDR-20
 - MDR-30
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 - RR-60
 - SC-G



ZO-2

MAP SHEET 2 of 5





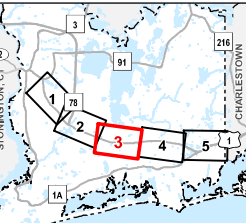
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Zoning and Overlay Areas

DECEMBER 2021

Data Sources:
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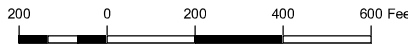
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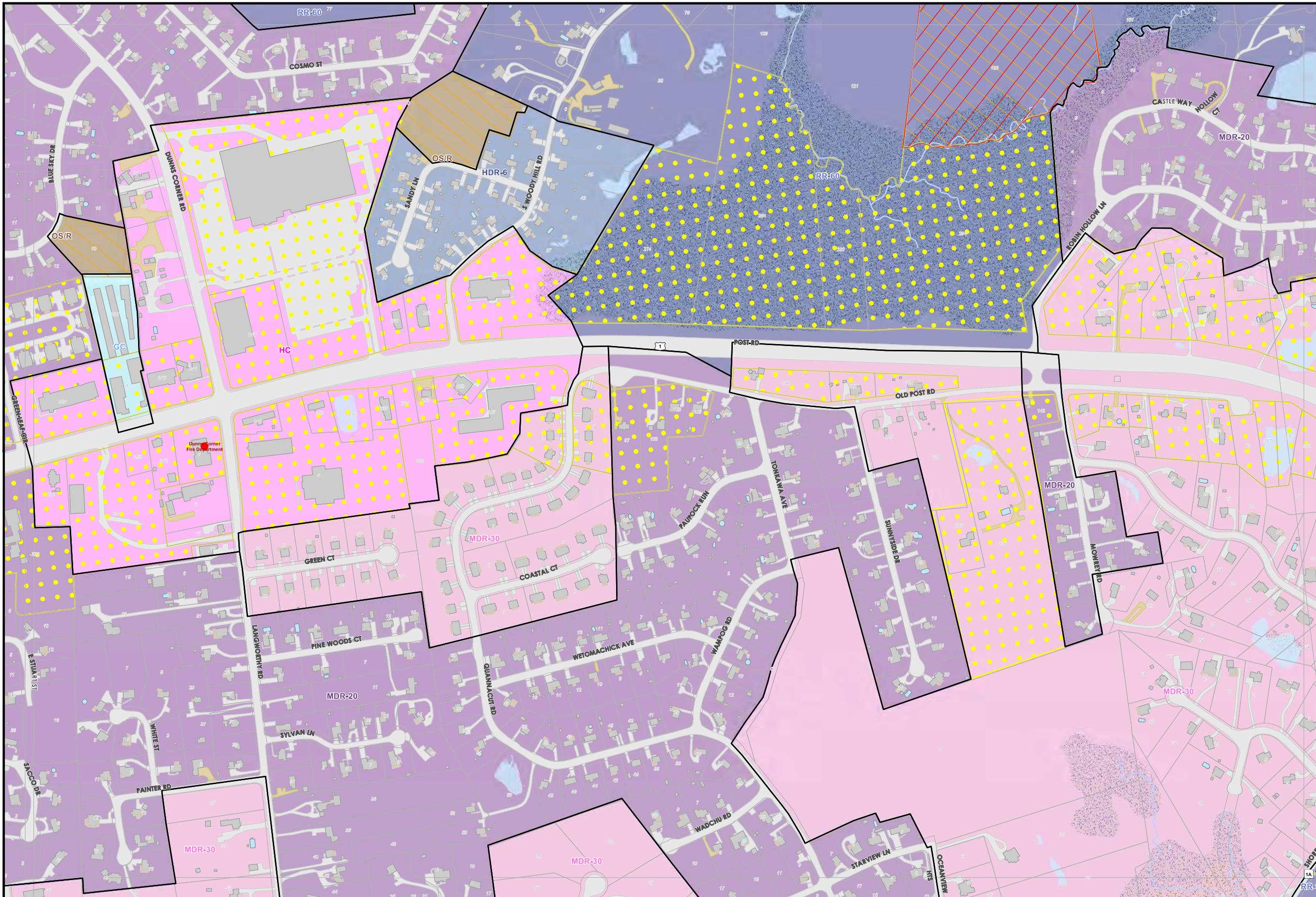
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- Parcels
 - State Conservation Areas
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 - Oversite/State "Conservation Intent"
 - Wilcox Park District
 - Down Town Historic District
 - Wells Street District
 - Granite Street District
 - Bed & Breakfast District
- Zoning**
- CR
 - DC-1
 - DC-2
 - CG
 - GI
 - HC
 - HDR-15
 - HDR-6
 - LI
 - MDR-20
 - MDR-30
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 - OS/R
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 - PUD
 - R-6
 - RR-60
 - SC-G



ZO-3

MAP SHEET 3 of 5





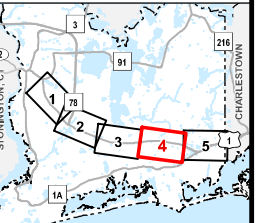
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Zoning and Overlay Areas

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

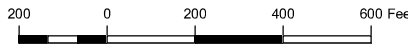
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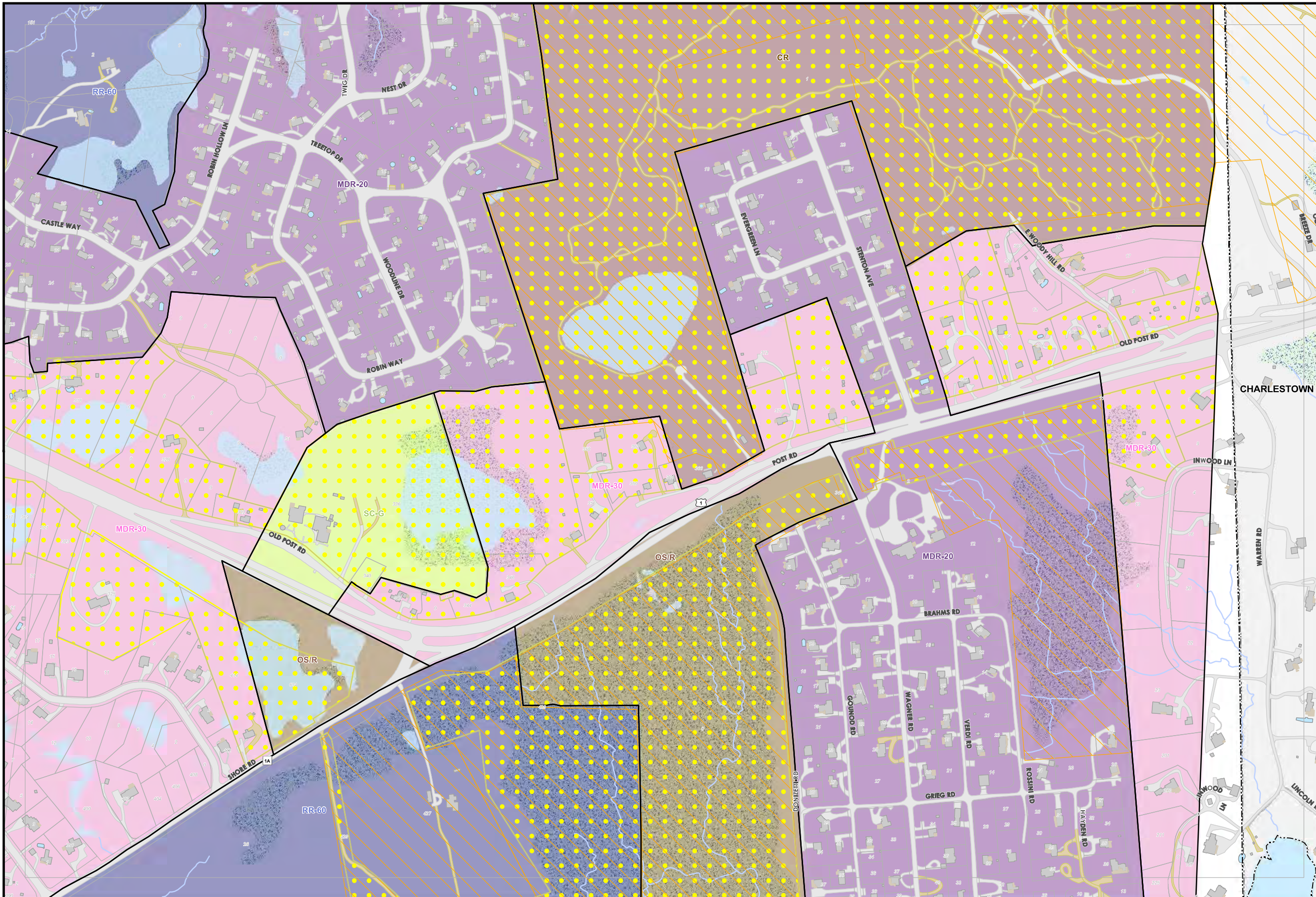
- Legend**
- Parcels
 - State Conservation Areas
 - Local Conservation Areas
 - Owner-Related "Conservation Intent"
 - Wilcox Park District
 - Down Town Historic District
 - Wells Street District
 - Granite Street District
 - Bed & Breakfast District
- Zoning**
- CR
 - OC-1
 - OC-II
 - CG
 - GI
 - HC
 - HDR-15
 - HDR-6
 - LI
 - MDR-20
 - MDR-30
 - MP
 - ORAT
 - OSIR
 - P-15
 - PUD
 - R-6
 - RR-60
 - SC-G



ZO-4

MAP SHEET 4 of 5





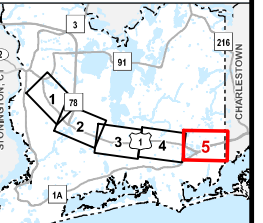
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Zoning and Overlay Areas

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIEGIS) and Town of Westerly Rhode Island.

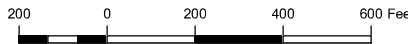
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for legal purposes. Field inspection and verification of all information is required. All information was created from schematic maps.

- Legend**
- Parcels
 - State Conservation Areas
 - Local Conservation Areas
 - Owner-stated "Conservation Intent"
 - Wilcox Park District
 - Down Town Historic District
 - Wells Street District
 - Granite Street District
 - Bed & Breakfast District
- Zoning**
- CR
 - DC-1
 - DC11
 - CG
 - CI
 - HC
 - HDR-15
 - HDR-6
 - L
 - MDR-20
 - MDR-30
 - NB
 - ORAT
 - OS/R
 - P-15
 - PUD
 - R-6
 - RR-60
 - SC-G



ZO-5

MAP SHEET 5 of 5



APPENDIX C

Airport Overlay Map Set



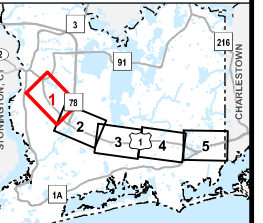
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Airport Overlay Areas

DECEMBER 2021

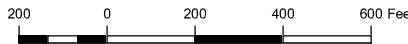
Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

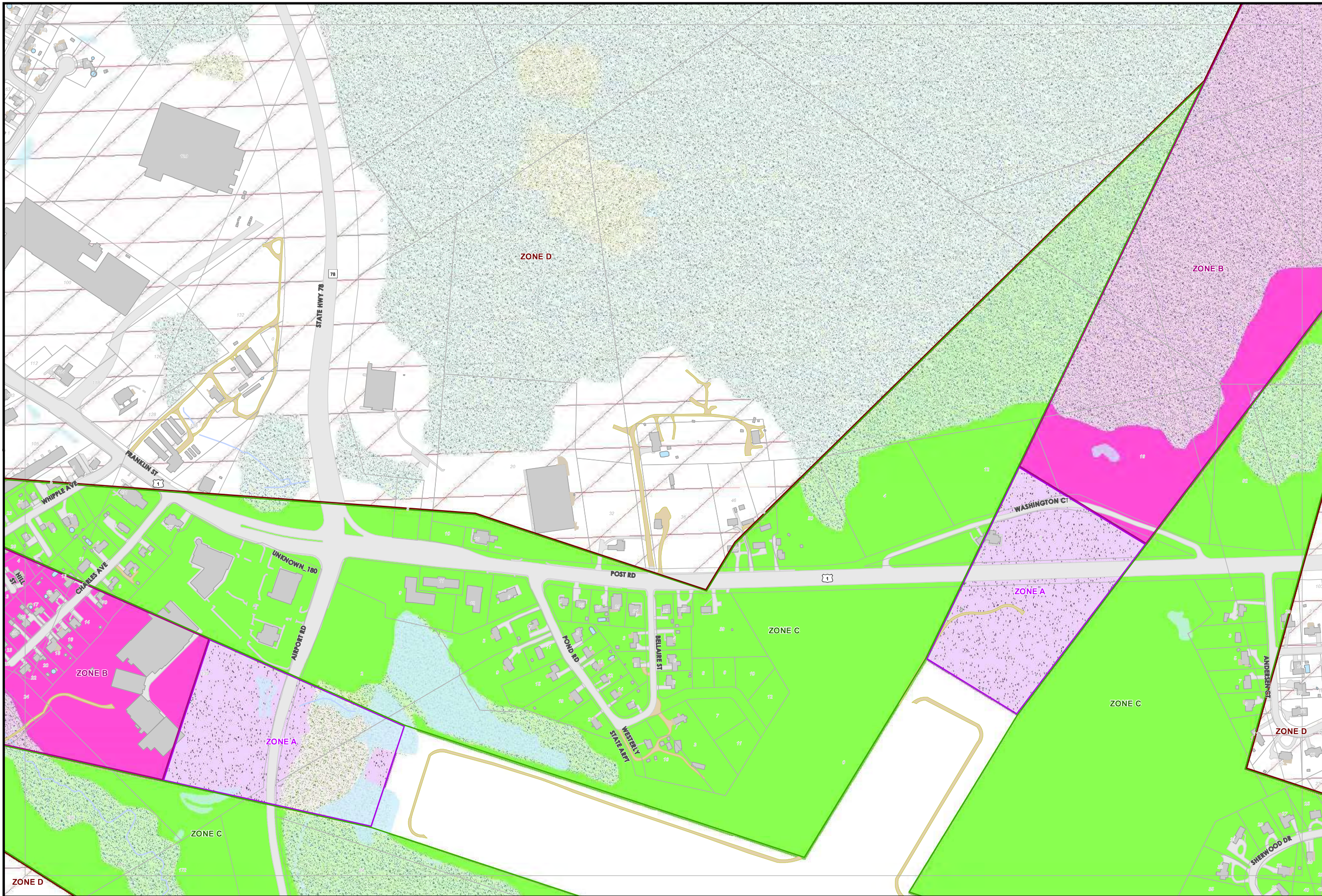
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for legal purposes. Field inspection and verification of data is required. This map was created from schematic maps.

- Legend**
- Parcels
 - Building
 - Deck
 - Swimming Pool
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands
 - Airport Zones
 - ZONE A (RPZ)
 - ZONE B (Approach Zone)
 - ZONE C (Transitional Zone)
 - ZONE D
 - ZONE E



AO-1
 MAP SHEET 1 of 5





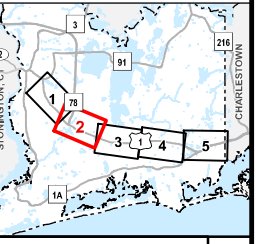
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Airport Overlay Areas

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RI GIS) and Town of Westerly Rhode Island.

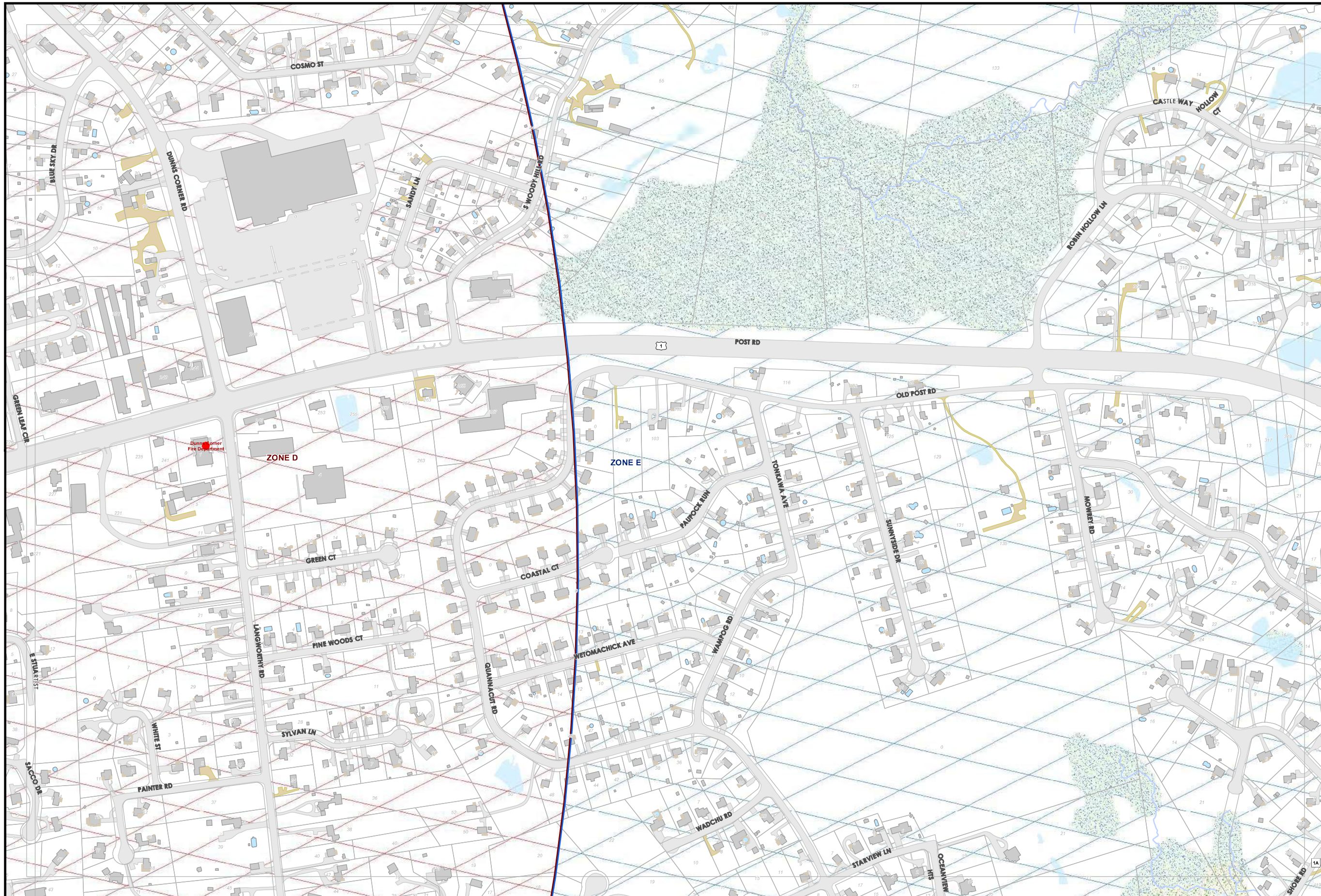
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for any other purpose. Field inspection and verification are required. This map was created from schematic maps.

- Legend**
- Parcels
 - Building
 - Deck
 - Swimming Pool
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands
 - Airport Zones**
 - ZONE A (RPZ)
 - ZONE B (Approach Zone)
 - ZONE C (Transitional Zone)
 - ZONE D
 - ZONE E



A0-2
 MAP SHEET 2 of 5





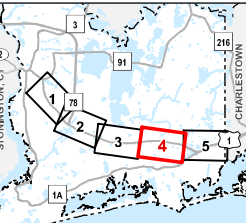
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Airport Overlay Areas

DECEMBER 2021

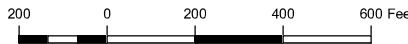
Data Sources:
 Data from Rhode Island Geographic Information System (RI&ES) and Town of Westerly Rhode Island.

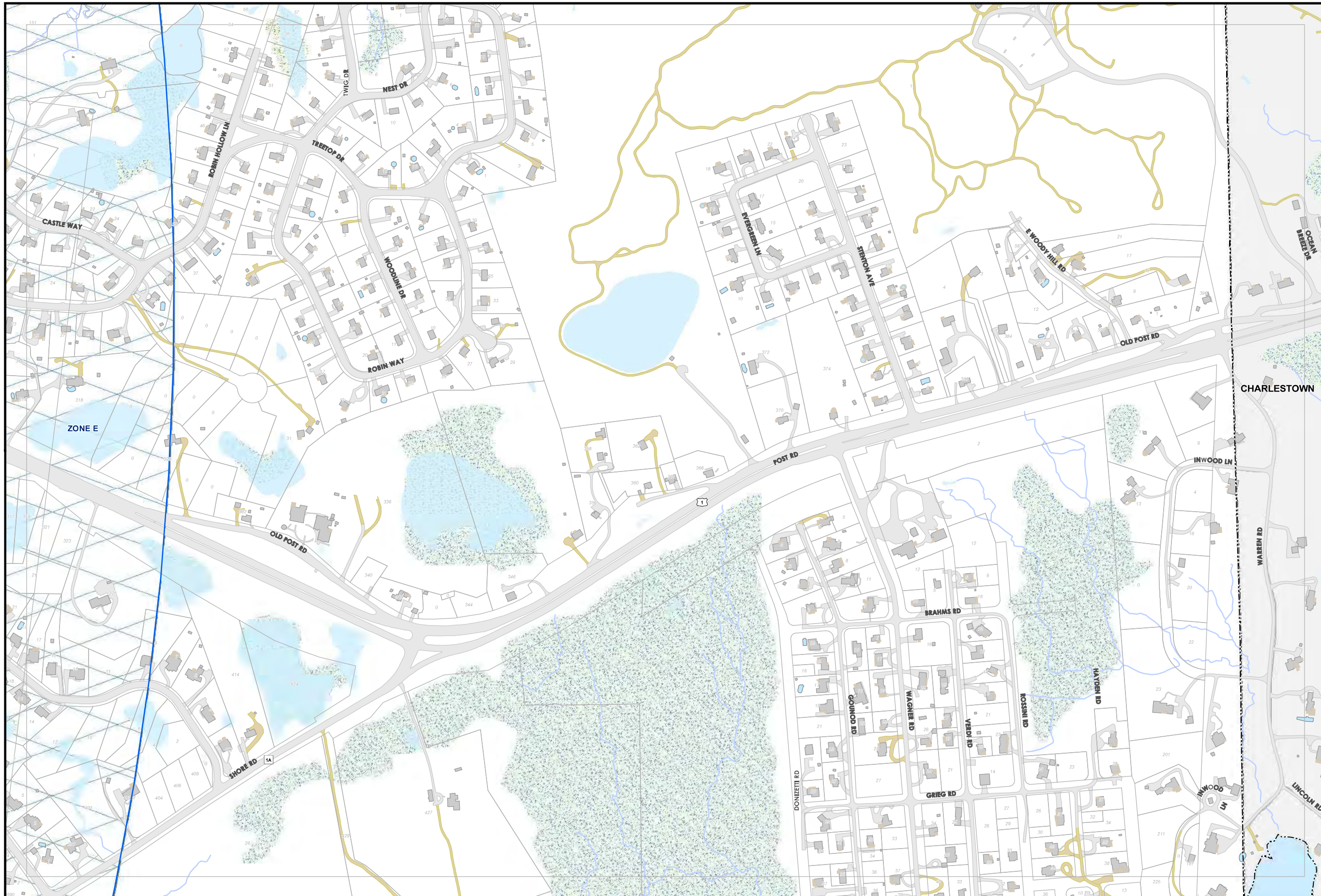
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for legal or engineering purposes. Field inspection and verification are required. The information was created from schematic maps.

- Legend**
- Parcels
 - Building
 - Deck
 - Swimming Pool
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Pasturelands
 - Airport Zones**
 - ZONE A (RPZ)
 - ZONE B (Approach Zone)
 - ZONE C (Transitional Zone)
 - ZONE D
 - ZONE E



AO-4
 MAP SHEET 4 of 5





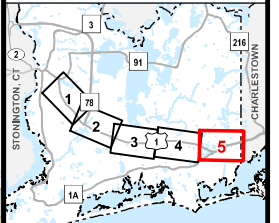
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Airport Overlay Areas

DECEMBER 2021

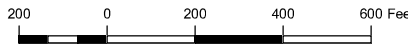
Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerlo Rhode Island.

Disclaimer:
 This information is for planning purposes only. It is not intended to be used for any other purpose. Field inspection and verification are required. All information was created from schematic maps.

- Legend**
- Parcels
 - Building
 - Deck
 - Swimming Pool
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Pastlands
 - Airport Zones**
 - ZONE A (RPZ)
 - ZONE B (Approach Zone)
 - ZONE C (Transitional Zone)
 - ZONE D
 - ZONE E



A0-5
 MAP SHEET 5 of 5



APPENDIX D

Land Use Map Set



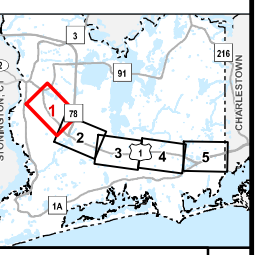
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Land Use Land Cover (2011)

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

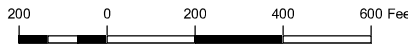
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for any other purpose. Field inspection and verification of the data is recommended. The map was created from schematic maps.

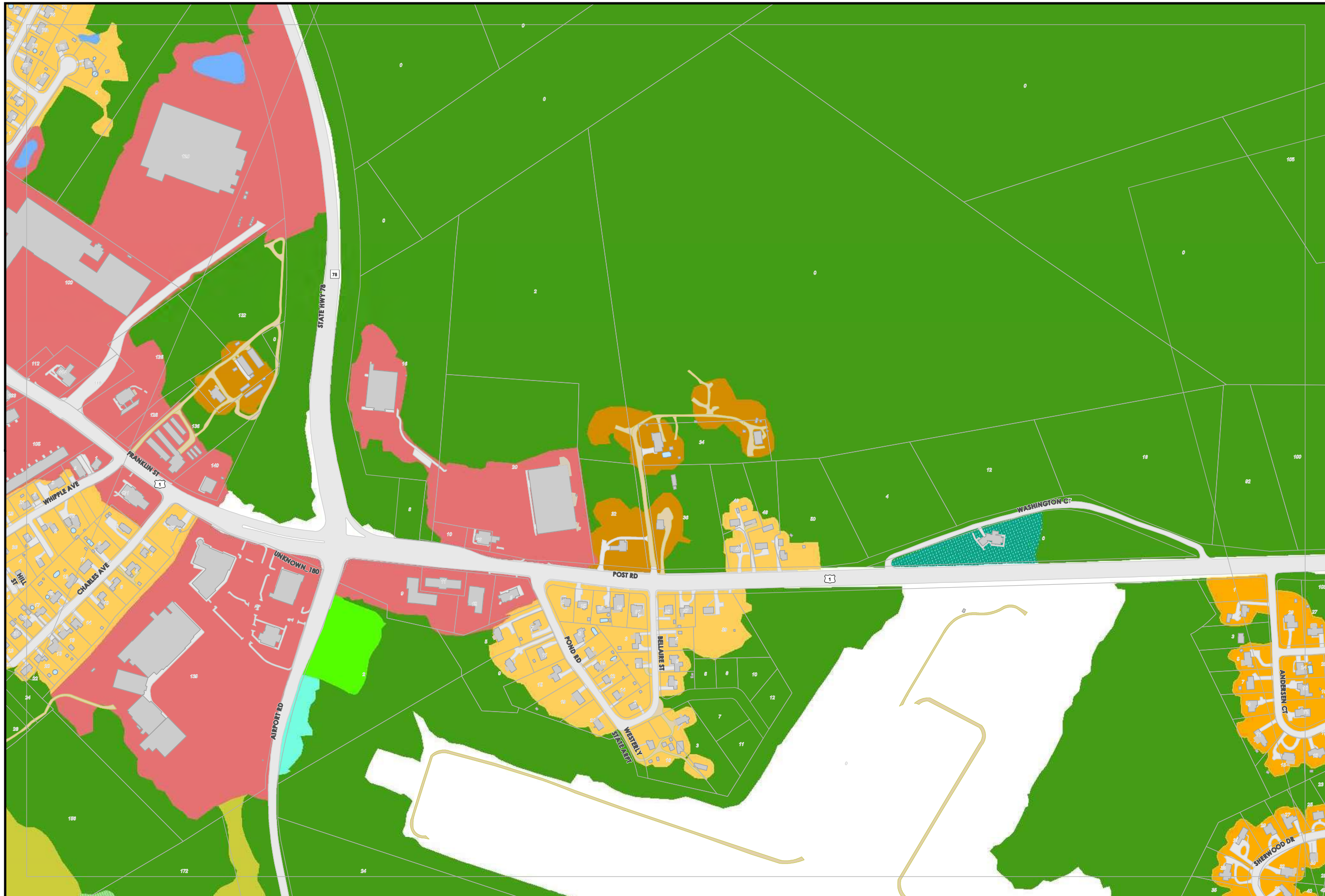
- Legend**
- Land Use Land Cover 2011**
- High Density Residential (<1/8 acre lots)
 - Medium High Density Residential (1/4 to 1/8 acre lots)
 - Medium Density Residential (1 to 1/4 acre lots)
 - Medium Low Density Residential (1 to 2 acre lots)
 - Low Density Residential (>2 acre lots)
 - Commercial (sale of products and services)
 - Industrial (manufacturing, design, assembly, etc.)
 - Water and Sewage Treatment
 - Waste Disposal (landfills, junkyards, etc.)
 - Power Lines (100' or more width)
 - Other Transportation (terminals, docks, etc.)
 - Developed Recreation (all recreation)
 - Vacant Land
 - Cemeteries
 - Institutional (schools, hospitals, churches, etc.)
 - Pasture (agricultural not suitable for tillage)
 - Cropland (tillable)
 - Orchards, Groves, Nurseries
 - Idle Agriculture (abandoned fields and orchards)
 - Brushland (shrub and brush areas, reforestation)
 - Forest
 - Water
 - Wetland
 - Mines, Quarries and Gravel Pits
 - Transitional Areas (urban open)
 - Mixed Barren Areas



LU-1

MAP SHEET 1 of 5





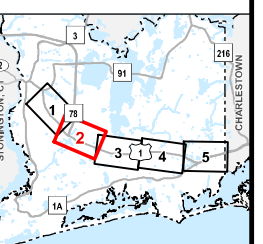
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Land Use Land Cover (2011)

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

Disclaimer:
 This information is for planning purposes only. It is not intended to be used for legal purposes. Field inspection and verification of data is required. This map was created from schematic maps.

- Legend**
- Land Use Land Cover 2011**
- High Density Residential (<1/8 acre lots)
 - Medium High Density Residential (1/4 to 1/8 acre lots)
 - Medium Density Residential (1 to 1/4 acre lots)
 - Medium Low Density Residential (1 to 2 acre lots)
 - Low Density Residential (>2 acre lots)
 - Commercial (sale of products and services)
 - Industrial (manufacturing, design, assembly, etc.)
 - Water and Sewage Treatment
 - Waste Disposal (landfills, junkyards, etc.)
 - Power Lines (100' or more width)
 - Other Transportation (terminals, docks, etc.)
 - Developed Recreation (all recreation)
 - Vacant Land
 - Cemeteries
 - Institutional (schools, hospitals, churches, etc.)
 - Pasture (agricultural not suitable for tillage)
 - Cropland (tillable)
 - Orchards, Groves, Nurseries
 - Agriculture (abandoned fields and orchards)
 - Brushland (shrub and brush areas, reforestation)
 - Forest
 - Water
 - Wetland
 - Mines, Quarries and Gravel Pits
 - Transitional Areas (urban open)
 - Mixed Barren Areas



LU-2

MAP SHEET 2 of 5





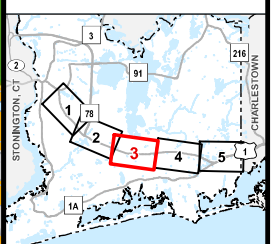
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Land Use Land Cover (2011)

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

Disclaimer:
 This information is for planning purposes only. It is not intended to be used for any other purpose. Field inspection and verification of data is required. This map was created from schematic maps.

- Legend**
Land Use Land Cover 2011
- High Density Residential (<1/8 acre lots)
 - Medium High Density Residential (1/4 to 1/8 acre lots)
 - Medium Density Residential (1 to 1/4 acre lots)
 - Medium Low Density Residential (1 to 2 acre lots)
 - Low Density Residential (>2 acre lots)
 - Commercial (sale of products and services)
 - Industrial (manufacturing, design, assembly, etc.)
 - Water and Sewage Treatment
 - Waste Disposal (landfills, junkyards, etc.)
 - Power Lines (100' or more width)
 - Other Transportation (terminals, docks, etc.)
 - Developed Recreation (all recreation)
 - Vacant Land
 - Cemeteries
 - Institutional (schools, hospitals, churches, etc.)
 - Pasture (agricultural not suitable for tillage)
 - Cropland (tillable)
 - Orchards, Groves, Nurseries
 - Idle Agriculture (abandoned fields and orchards)
 - Brushland (shrub and brush areas, reforestation)
 - Forest
 - Water
 - Wetland
 - Mines, Quarries and Gravel Pits
 - Transitional Areas (urban open)
 - Mixed Barren Areas



LU-3
 MAP SHEET 3 of 5



PROJECT 152525 - WESTERLY, RHODE ISLAND - ROUTE 1 CORRIDOR - LAND USE AND LAND COVER MAP - 12/2021



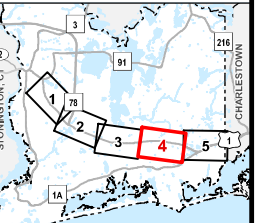
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Land Use Land Cover (2011)

DECEMBER 2021

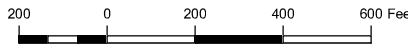
Data Sources:
 Data from Rhode Island Geographic Information System (RIERS) and Town of Westerly Rhode Island.

Disclaimer:
 This information is for planning purposes only. It is not intended to be used for legal purposes. Field inspection and verification of data is required. All data was created from schematic maps.

- Legend**
Land Use Land Cover 2011
- High Density Residential (<1/8 acre lots)
 - Medium High Density Residential (1/4 to 1/8 acre lots)
 - Medium Density Residential (1 to 1/4 acre lots)
 - Medium Low Density Residential (1 to 2 acre lots)
 - Low Density Residential (>2 acre lots)
 - Commercial (sale of products and services)
 - Industrial (manufacturing, design, assembly, etc.)
 - Water and Sewage Treatment
 - Waste Disposal (landfills, junkyards, etc.)
 - Power Lines (100' or more width)
 - Other Transportation (terminals, docks, etc.)
 - Developed Recreation (all recreation)
 - Vacant Land
 - Cemeteries
 - Institutional (schools, hospitals, churches, etc.)
 - Pasture (agricultural not suitable for tillage)
 - Cropland (tillable)
 - Orchards, Groves, Nurseries
 - Idle Agriculture (abandoned fields and orchards)
 - Brushland (shrub and brush areas, reforestation)
 - Forest
 - Water
 - Wetland
 - Mines, Quarries and Gravel Pits
 - Transitional Areas (urban open)
 - Mixed Barren Areas



LU-4
 MAP SHEET 4 of 5





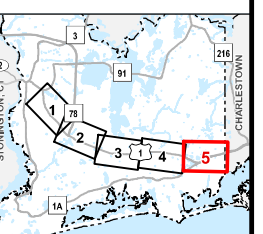
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Land Use Land Cover (2011)

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

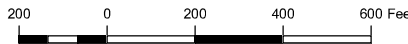
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for legal purposes. Field inspection and verification of the data is recommended. The data was created from schematic maps.

- Legend**
- Land Use Land Cover 2011**
- High Density Residential (<1/8 acre lots)
 - Medium High Density Residential (1/4 to 1/8 acre lots)
 - Medium Density Residential (1 to 1/4 acre lots)
 - Medium Low Density Residential (1 to 2 acre lots)
 - Low Density Residential (>2 acre lots)
 - Commercial (sale of products and services)
 - Industrial (manufacturing, design, assembly, etc.)
 - Water and Sewage Treatment
 - Waste Disposal (landfills, junkyards, etc.)
 - Power Lines (100' or more width)
 - Other Transportation (terminals, docks, etc.)
 - Developed Recreation (all recreation)
 - Vacant Land
 - Cemeteries
 - Institutional (schools, hospitals, churches, etc.)
 - Pasture (agricultural not suitable for tillage)
 - Cropland (tillable)
 - Orchards, Groves, Nurseries
 - Idle Agriculture (abandoned fields and orchards)
 - Brushland (shrub and brush areas, reforestation)
 - Forest
 - Water
 - Wetland
 - Mines, Quarries and Gravel Pits
 - Transitional Areas (urban open)
 - Mixed Barren Areas



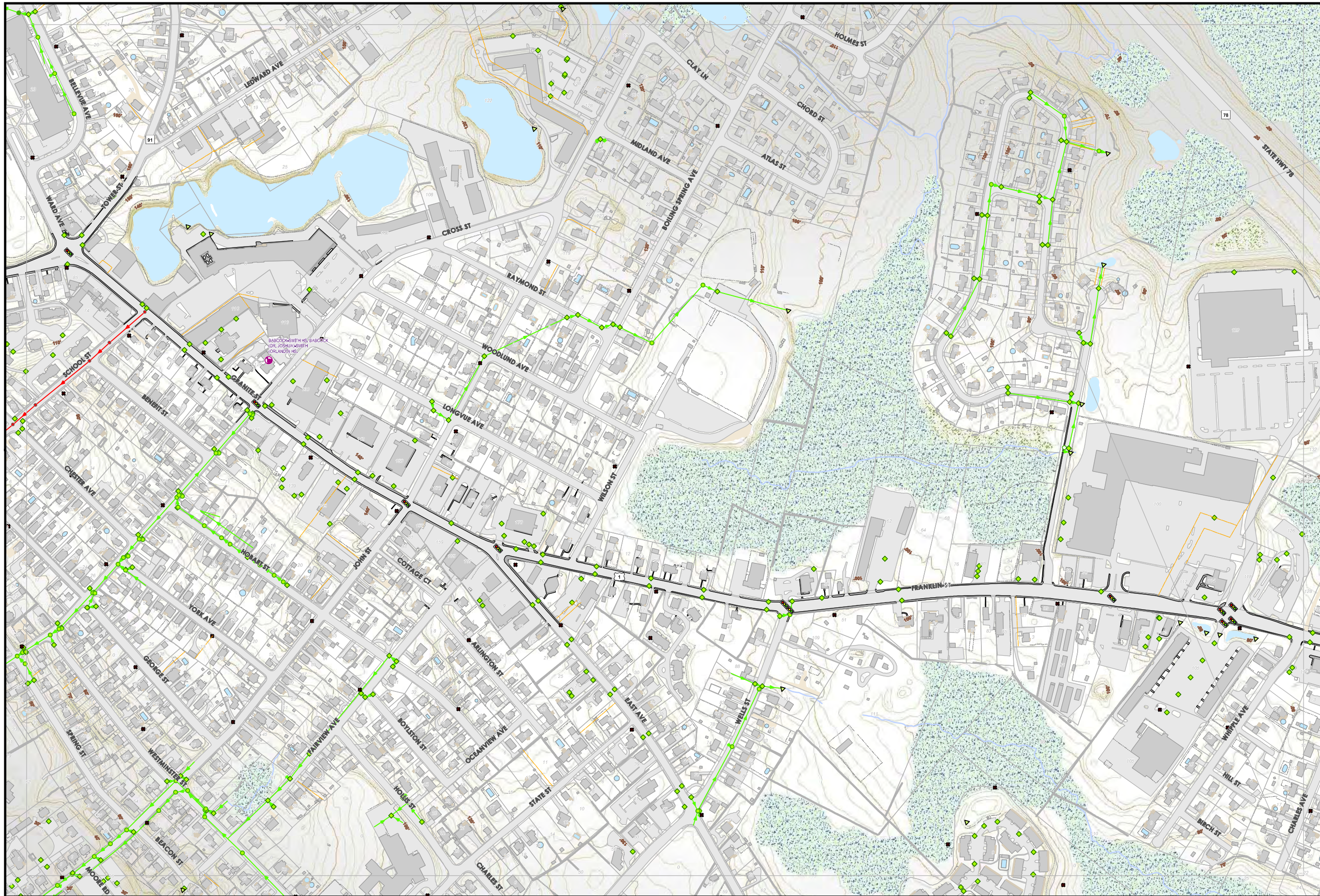
LU-5

MAP SHEET 5 of 5



APPENDIX E

Infrastructure Map Set



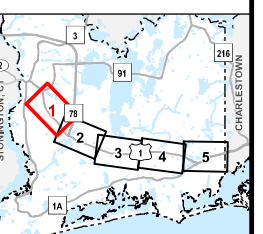
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Infrastructure

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

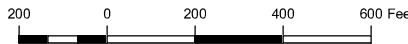
Disclaimer:
 This information is for planning purposes only. Field inspection and verification of all data is required. All data was created from schematic maps.

- Legend**
- Traffic Signal
 - Essements
 - Hydrants
 - Sewer Manhole
 - Pump Station
 - Gravity Sewer
 - Low Pressure Sewer
 - Low Pressure
 - Force Main
 - Outfall
 - Catch Basin
 - Closed Lid Manhole
 - Open Lid Manhole
 - Other Inlet
 - Drain Gravity Main
 - Parcels
 - Fence
 - Retaining Wall
 - Rock Wall
 - Wall
 - Building
 - Deck
 - Swimming Pool
 - Sidewalk, Paved
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands
 - Index Contour
 - Intermediate Contour

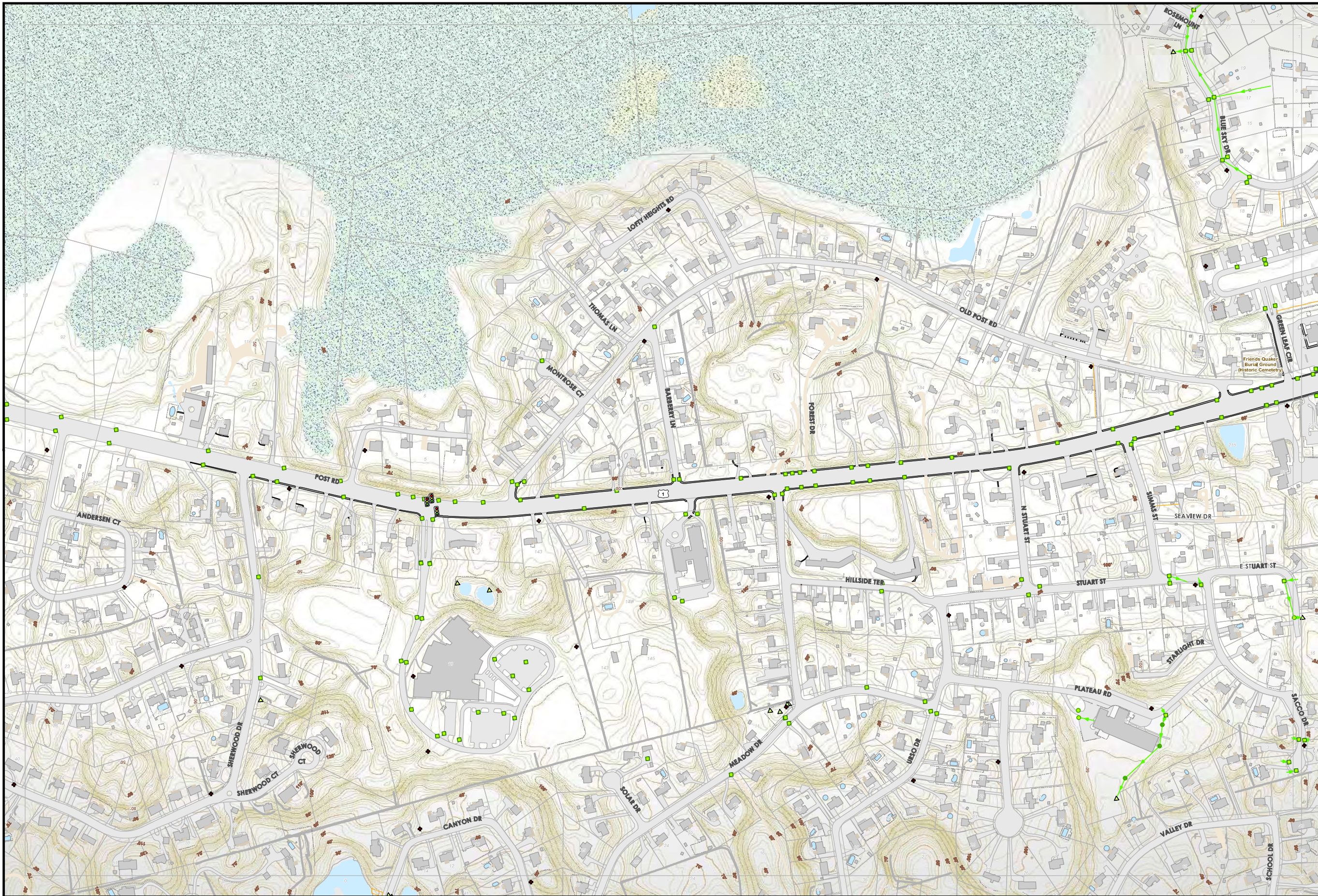


INF-1

MAP SHEET 1 of 5



PROJECT: 20210101-0001, DATE: 12/15/2021, 3:00PM, LOCATION: Westerly, Rhode Island, WESTERLY, RHODE ISLAND, WESTERLY CORRIDOR INFRASTRUCTURE



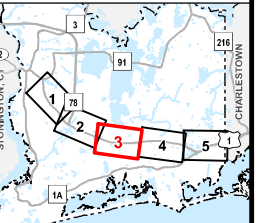
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
Infrastructure

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RI GIS) and Town of Westerly Rhode Island.

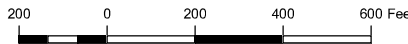
Disclaimer:
 This information is for planning purposes only. It is not intended to be used for construction or other purposes. Field inspection and verification are required. The information was created from schematic maps.

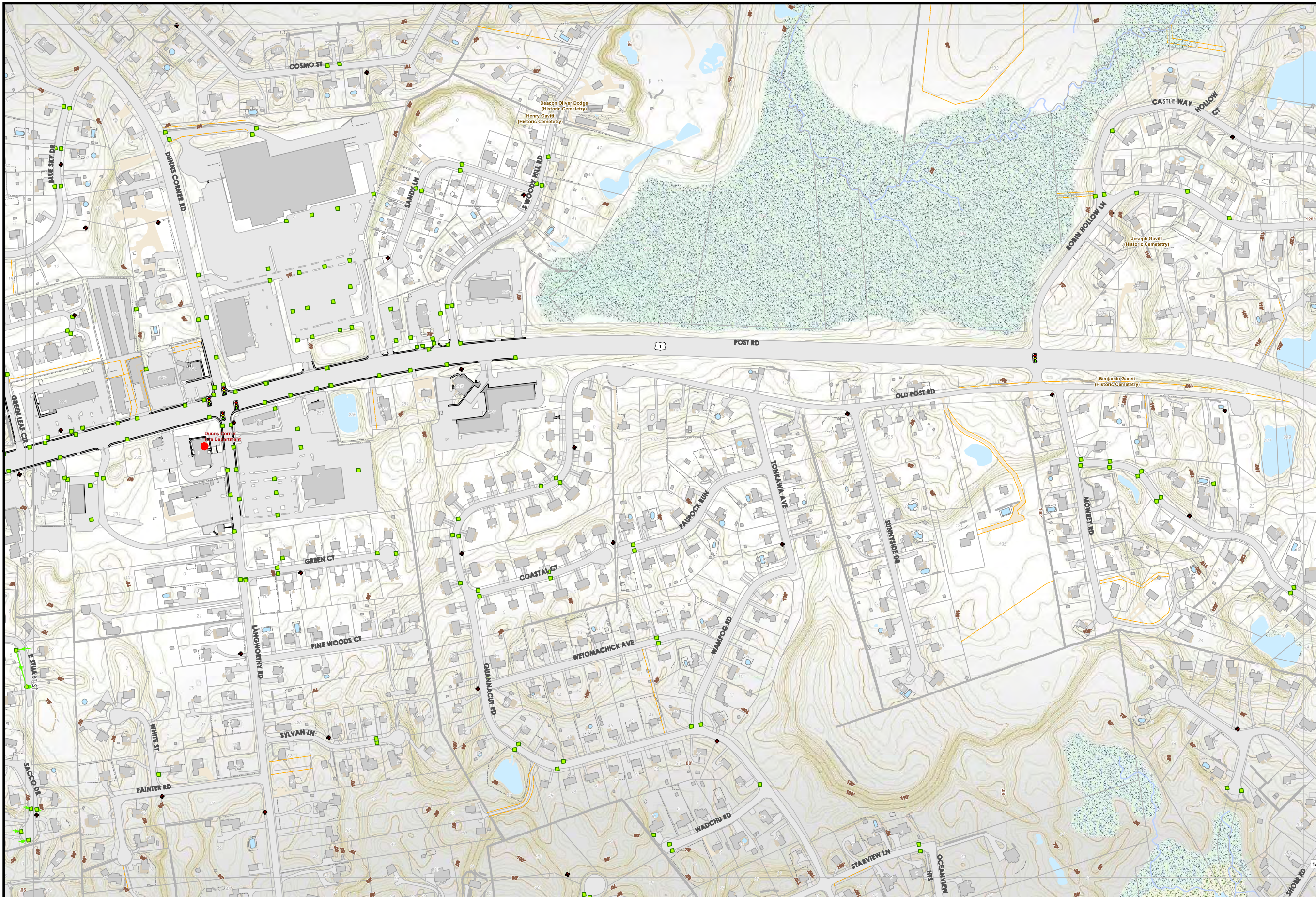
- Legend**
- Traffic Signal
 - Essements
 - Hydrants
 - Sewer Manhole
 - Pump Station
 - Gravity Sewer
 - Low Pressure Sewer
 - Low Pressure
 - Forcmain
 - Outfall
 - Catch Basin
 - Closed Lid Manhole
 - Open Lid Manhole
 - Other Inlet
 - Drain Gravity Main
 - Parcels
 - Fence
 - Retaining Wall
 - Rock Wall
 - Wall
 - Building
 - Deck
 - Swimming Pool
 - Sidewalk, Paved
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands
 - Index Contour
 - Intermediate Contour



INF-3

MAP SHEET 3 of 5





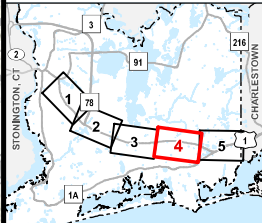
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Infrastructure

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RI GIS) and Town of Westerly Rhode Island.

Disclaimer:
 This information is for planning purposes only. It is not intended for field inspection and verification. It is not a substitute for a professional engineer's design. It was created from schematic maps.

- Legend**
- Traffic Signal
 - Easements
 - Hydrants
 - Sewer Manhole
 - Pump Station
 - Gravity Sewer
 - Low Pressure Sewer
 - Low Pressure
 - Foremain
 - Outfall
 - Catch Basin
 - Closed Lid Manhole
 - Open Lid Manhole
 - Other Inlet
 - Drain Gravity Main
 - Parcels
 - Fence
 - Retaining Wall
 - Rock Wall
 - Wall
 - Building
 - Deck
 - Swimming Pool
 - Sidewalk, Paved
 - Paved Surface
 - Unpaved Surface
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands
 - Index Contour
 - Intermediate Contour



INF-4
 MAP SHEET 4 of 5





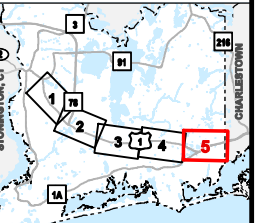
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Infrastructure

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

Disclaimer:
 This information is for planning purposes only. Field inspections and verification are recommended. Do not rely on this information for any other purpose. Information was derived from authoritative maps.

- Legend**
- Traffic Signal
 - Easements
 - Hydrants
 - Sewer Manhole
 - Pump Station
 - Gravty Sewer
 - Low Pressure Sewer
 - Low Pressure
 - Foremain
 - Outfall
 - Catch Basin
 - Closed Lid Manhole
 - Open Lid Manhole
 - Other Inlet
 - Drain Gravty Main
 - Parcels
 - Fence
 - Retaining Wall
 - Rock Wall
 - Wall
 - Building
 - Deck
 - Swimming Pool
 - Sidewalk, Paved
 - Paved Surface
 - Unpaved Surface
 - Stream
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands
 - Index Contour
 - Intermediate Contour



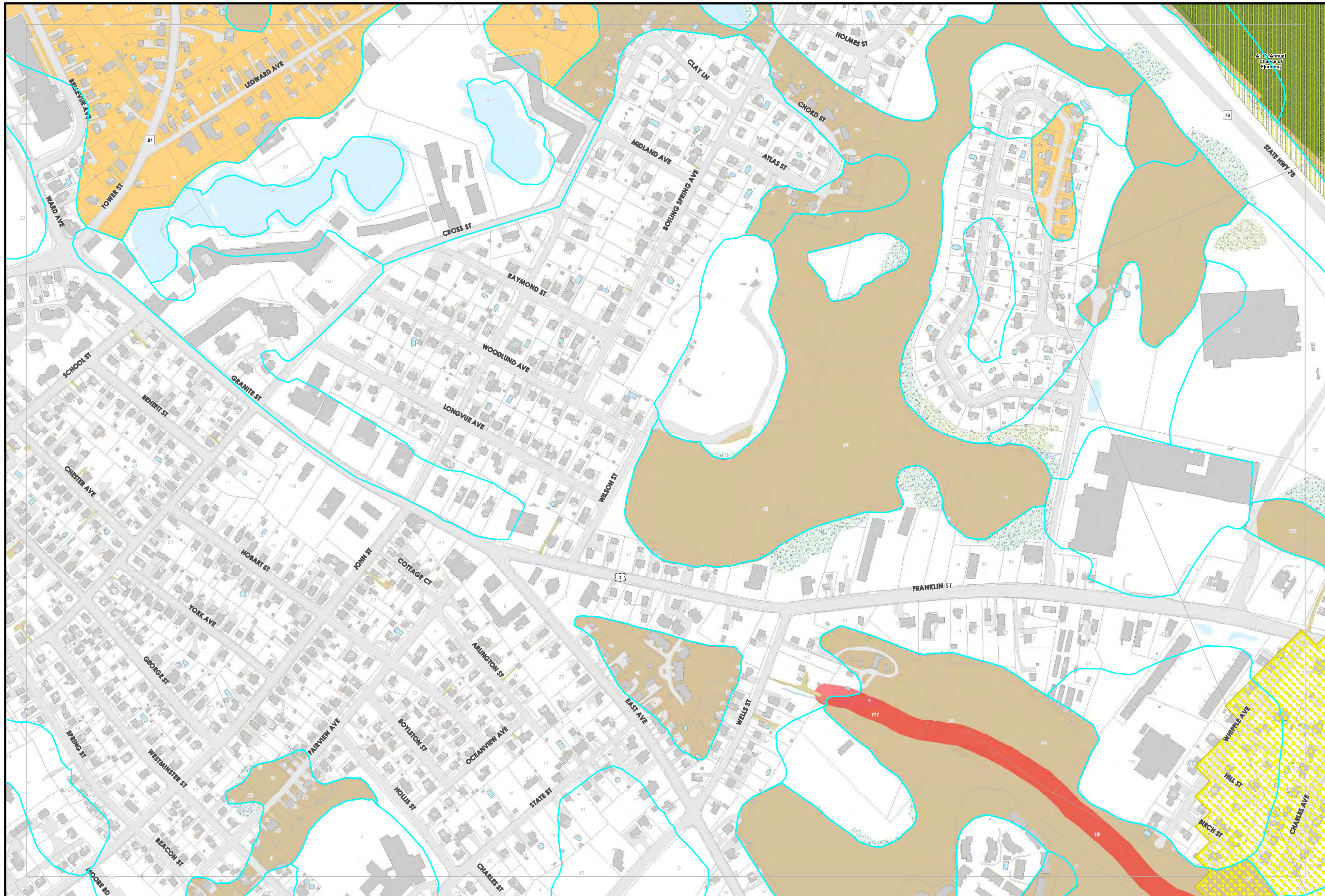
INF-5

MAP SHEET 5 of 5



APPENDIX F

Environmental Resource Areas



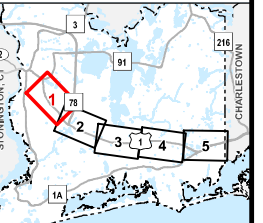
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Environmental Resource Areas

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

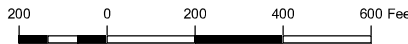
Disclaimer:
 This information is for planning purposes only. Field inspection and verification of all data is required. All data was created from schematic maps.

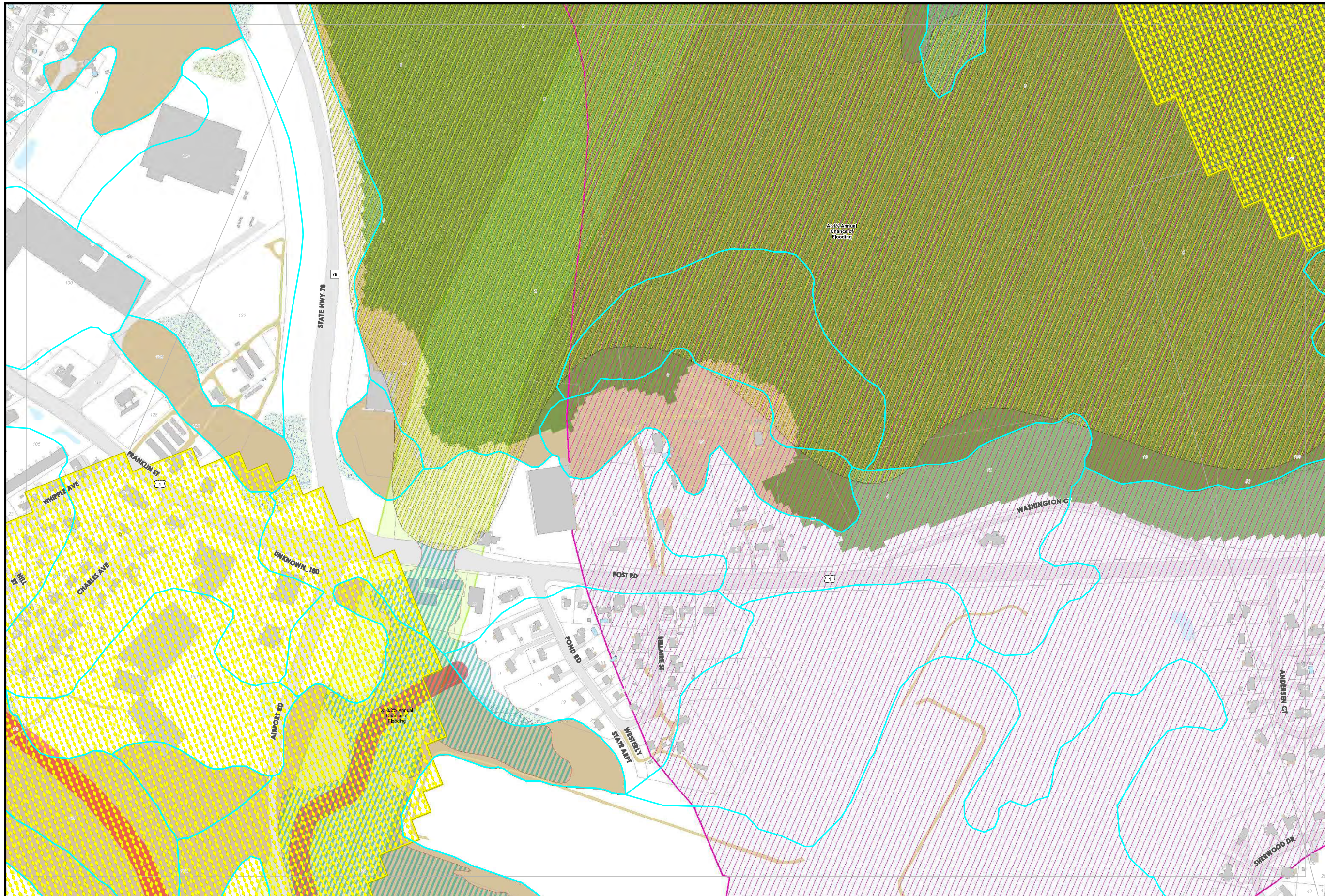
- Legend**
- Natural Heritage Areas
 - Flood Hazard Areas**
 - X: 0.2% Annual Chance of Flooding
 - A: 1% Annual Chance of Flooding
 - AE: 1% Annual Chance of Flooding
 - MHHW Plus SLR by 2100
 - Inundation**
 - MHHW Plus 1
 - MHHW Plus 3
 - MHHW Plus 5
 - MHHW Plus 7
 - Environmental Resource Areas**
 - Critical or Uncommon Habitat
 - Community Wellhead Protection Areas
 - Non-Community Wellhead Protection Areas (None in maps)
 - Unfragmented Forest Blocks (250 < 500 acres) (None in maps)
 - Unfragmented Forest Blocks (500 acres or more)
 - High Value / High Vulnerability Habitat
 - Corridor connecting contiguous Core Natural Areas identified by RIDEM
 - Ecological Land Unit (None in maps)
 - Groundwater Recharge Area (None in maps)
 - Hydrologic Group - D
 - Steep Slopes (15%+)
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands



EA-1

MAP SHEET 1 of 5





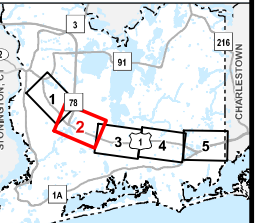
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Environmental Resource Areas

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

Disclaimer:
 This information is for planning purposes only. It is not intended to be used for any other purpose. Field inspection and verification of data is required. This map was created from schematic maps.

- Legend**
- Natural Heritage Areas
 - Flood Hazard Areas**
 - X: 0.2% Annual Chance of Flooding (None in maps)
 - A: 1% Annual Chance of Flooding
 - AE: 1% Annual Chance of Flooding
 - MHHW Plus SLR by 2100**
 - MHHW Plus 1
 - MHHW Plus 3
 - MHHW Plus 5
 - MHHW Plus 7
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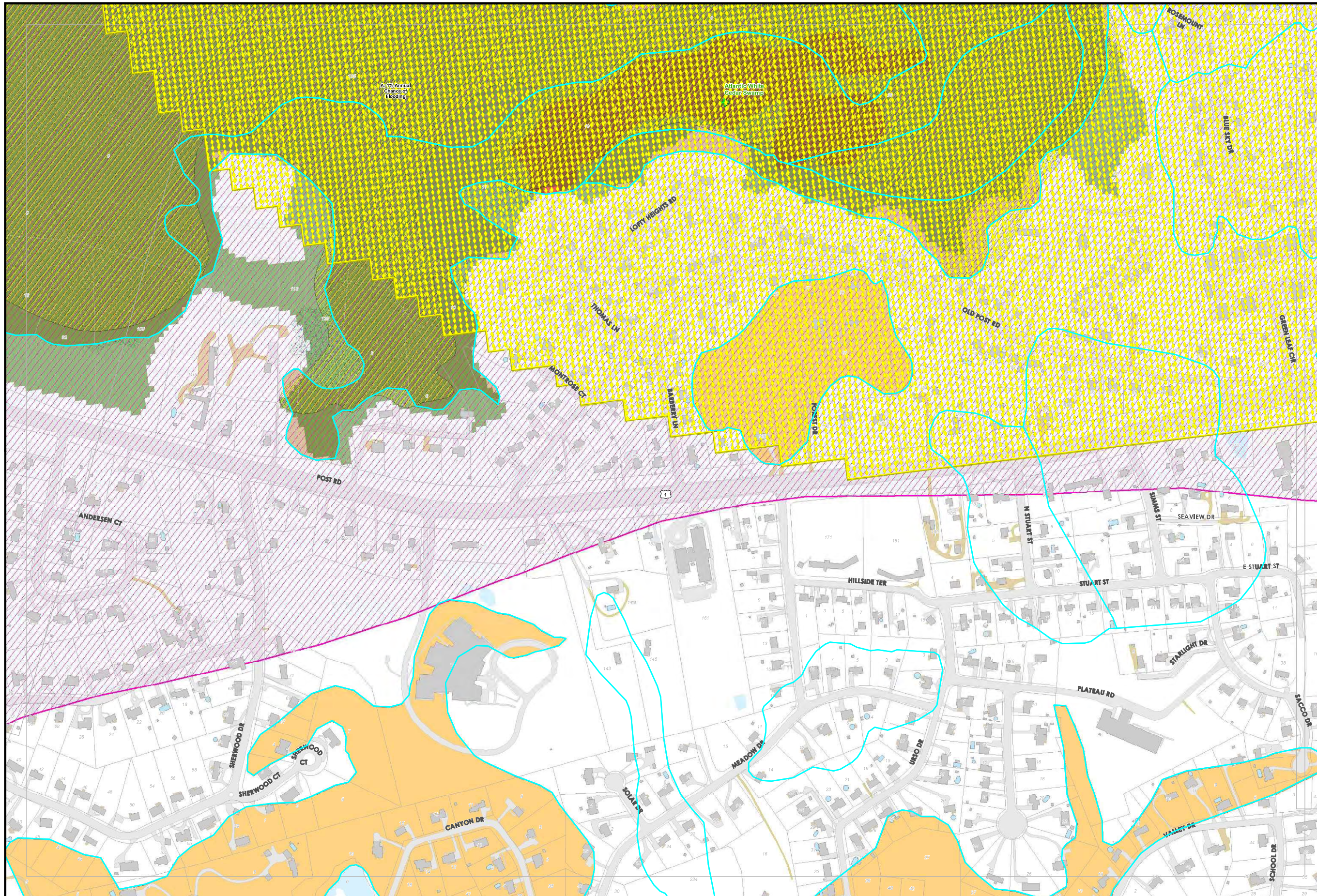


EA-2

MAP SHEET 2 of 5



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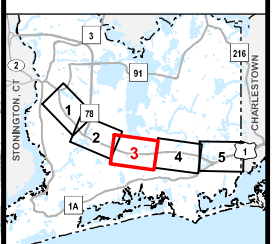
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Environmental Resource Areas

DECEMBER 2021

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Disclaimer:
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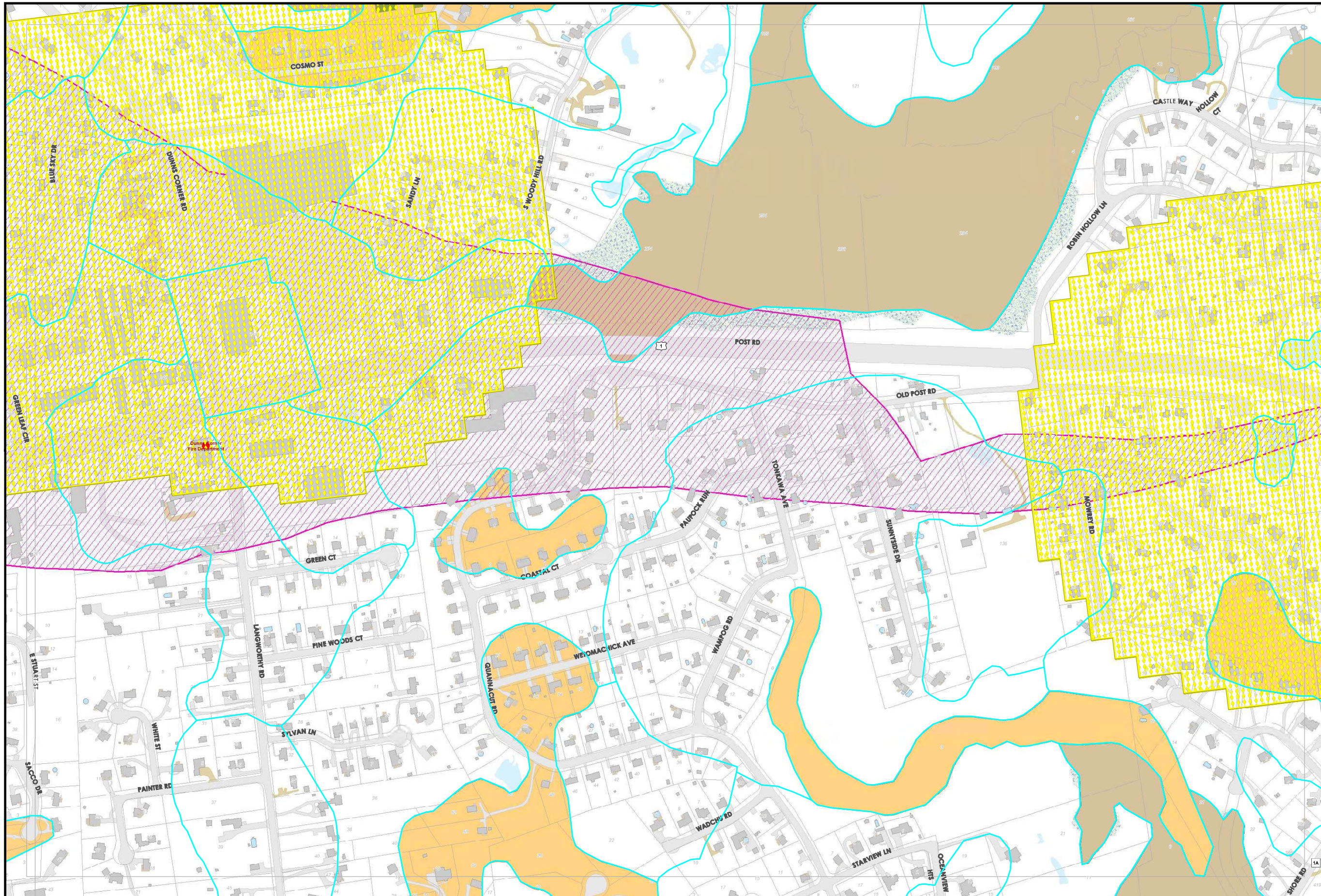
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 - AE: 1% Annual Chance of Flooding
 - MHHW Plus SLR by 2100
 - Inundation**
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 - Streams
 - Lakes & Ponds
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 - Open Peatlands



EA-3

MAP SHEET 3 of 5





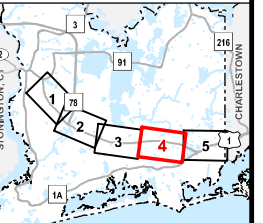
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Environmental Resource Areas

DECEMBER 2021

Data Sources:
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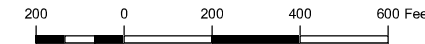
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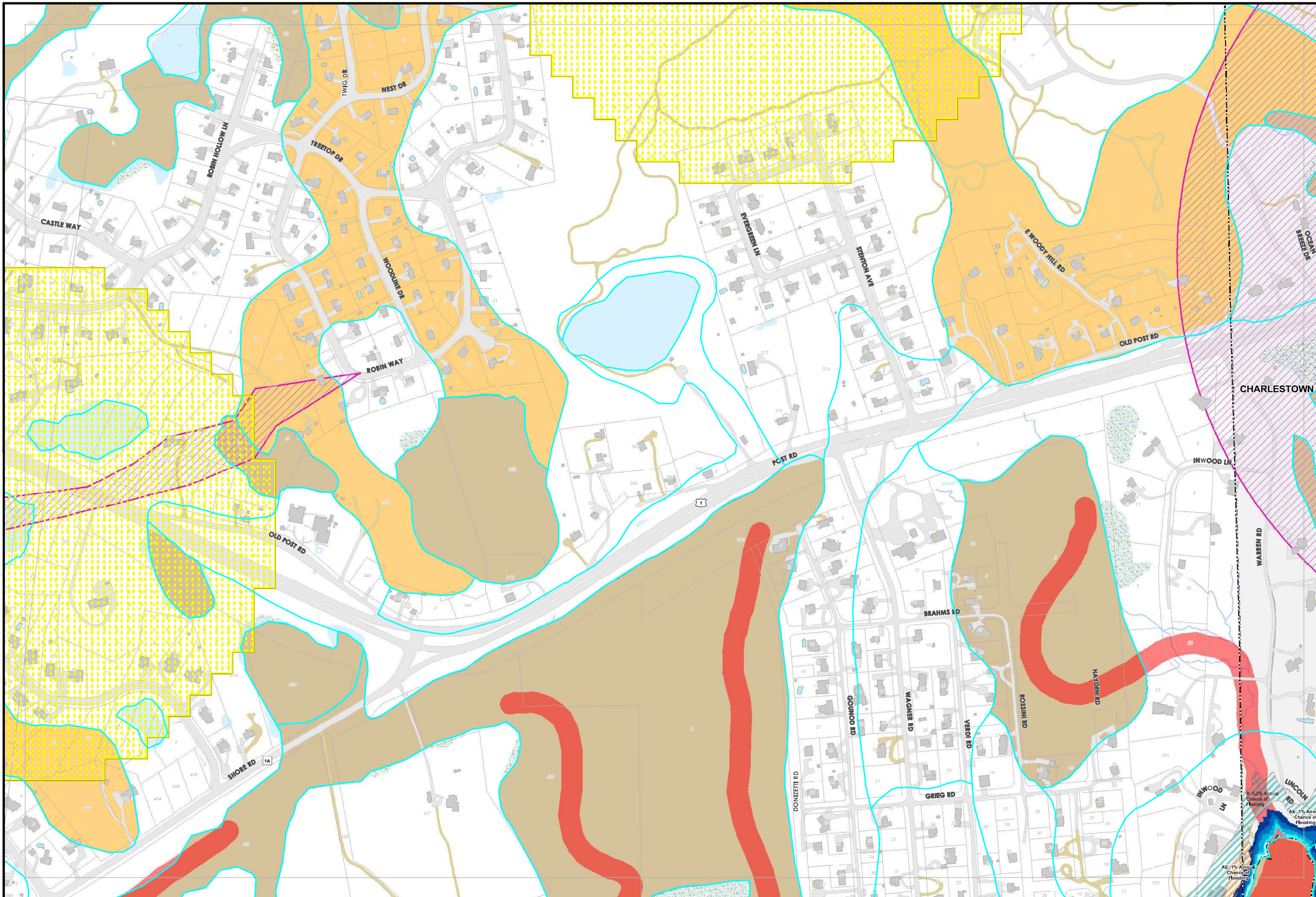
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 - Flood Hazard Areas**
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 - A: 1% Annual Chance of Flooding
 - AE: 1% Annual Chance of Flooding
 - MHW Plus SLR by 2100
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 - Steep Slopes (15%+)
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands



EA-4

MAP SHEET 4 of 5





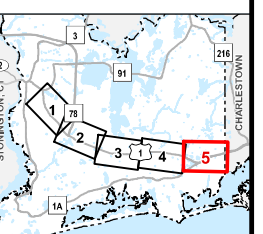
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
 Environmental Resource Areas

DECEMBER 2021

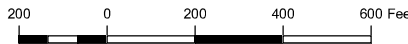
Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

Disclaimer:
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 - 1% Annual Chance of Flooding
 - AE: 1% Annual Chance of Flooding
 - MHHW Plus SLR by 2100
 - Inundation**
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 - Ecological Land Unit (None in maps)
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 - Hydrologic Group - D
 - Steep Slopes (15%+)
 - Streams
 - Lakes & Ponds
 - Forested Wetlands (Mineral and Peat Soils)
 - Open Mineral Soil Wetlands
 - Open Peatlands



EA-5
 MAP SHEET 5 of 5



APPENDIX G

RIDEM Summary of Draft Revisions to Freshwater Wetlands Regulations

DRAFT SUMMARY OF DRAFT REVISIONS TO STATE FRESHWATER WETLAND REGULATIONS

November 2020

Background

State law pertaining to freshwater wetlands (R.I. Gen. Laws §§ 2-1-18 through 2-1-28) was amended to strengthen the protection of freshwater wetland resources while streamlining the regulatory framework applicable to projects and activities proposed near wetlands. Implementation of the law required both the RI Department of Environmental Management (DEM) and the RI Coastal Resources Management Council (CRMC) to undertake rulemaking within their respective programs. The amended state law was based on the findings and recommendations of a Legislative Task Force (LTF) previously established by the Regulatory Reform Act (R.I. Gen. Laws § 42-64.13-10). The LTF was composed of a variety of stakeholders and charged with evaluating the adequacy of protection of Rhode Island freshwater wetlands considering both the state and municipal level, evaluating if gaps in that protection existed based on current scientific data and recommending changes in state law or regulations that could foster a business climate to grow the economy while ensuring better protection of our natural resources.

The amended state law acknowledged the important functions and values of freshwater wetlands and their buffers, the need to strengthen wetland protection and the need to protect and regulate the areas adjacent to wetlands. The law also recognized the benefits of having a single set of wetland protection standards administered only at the state level. Key provisions of the new law include:

- Strengthens wetland protection administered at the state level while eliminating the duplication of effort at the municipal level.
- Expands the jurisdiction of state agencies and requires the promulgation by rule of standards for freshwater wetland buffers and setbacks. The legislation recognizes the important values of buffers in the protection of the wetland resources.
- Re-defines and clarifies terminology and promotes common terminology among DEM and CRMC programs.
- Maintains the existing definition of “Farmer” and applicable regulatory procedures, as codified in state law, and associated permitting exemptions.
- Establishes new requirements for state agencies to share information with municipalities.

Draft Revised Freshwater Wetland Regulations

Overview: DEM and CRMC have drafted amendments to their respective existing freshwater wetland rules to fulfill the statutory mandates. Preliminary draft rules were the subject of a public workshop in September 2019. The September 2020 draft rules

reflect revisions by the Agencies in response to public feedback. The statutory changes in definitions affect the wording in all sections of the rules and result in a new framework for the regulatory process. Hence, the rulemaking involves repealing and replacing the rules currently in place. The major new changes involve the designation of “jurisdictional area” and specifying requirements for freshwater wetland buffers and setbacks within the jurisdictional area. To support regulatory process improvements, the draft rules also introduce a general permit process, propose new exempt activities, expand or clarify certain other exemptions and simplify the application fee schedule. The rules have been further restructured to have the performance standards and the review criteria listed in a single section 2.7. Portions of the rules have been restructured to improve organization for the reader but have not been substantively modified; e.g. review criteria and significant alteration procedure. Rules related to agricultural activities have been grouped into its own section 2.13 and rules related to coordination with municipalities have been specified and the existing provisions related to the municipal veto have been eliminated to be consistent with the amended state law.

Section 2.4 Definitions:

The draft rules include new or revised definitions required by state law and intended to provide greater consistency among DEM and CRMC programs as well as clarity for applicants and the public.

Freshwater wetlands: The definition of freshwater wetlands has been changed and now refers to the resources to be protected which includes vegetated wetlands and surface waters. The new definition is broadly applicable with a limited exception for certain farming activities. The terms “perimeter wetland” and “riverbank wetland” are no longer defined as wetlands and used in the rules. [§ 2.4(A)(32)]

Jurisdictional Area: This new term defines the land and waters that are subject to regulation and includes freshwater wetlands, buffers, floodplains, areas subject to storm flowage, areas subject to flooding and contiguous areas extending 200 outward from the edge of a river, stream or drinking water supply reservoir and 100 feet outward from all other wetlands. Persons planning new projects or regulated activities within the Jurisdictional Area will need to obtain a permit unless otherwise exempt. [§ 2.4(A)(39)]

Buffer Zone: This new term is used to refer to the land that is contiguous to a freshwater wetland and within which vegetated buffer should be maintained or in some situations created. Buffer zones are designated within or up to the limit of the jurisdictional area. Land within the buffer zones includes a range of land uses as well as areas qualifying as “buffer” (see below). [§ 2.4(A)(11)]

Buffer: This new term refers to an area of undeveloped vegetated land that is to be retained in its natural undisturbed condition. (Buffers can be created.) [§ 2.4(A)(10)]

Section 2.5 Applicability and Regulated Activities:

The amended law expanded the jurisdictional authority of the Agencies. (See definition of Jurisdictional Area above).

Section 2.6 Exempt Activities:

Within the Jurisdictional Area, the draft rules include the following new exemptions and expansion or clarification of approximately 16 other existing exemptions. All exemptions are subject to certain conditions and limitations specified in the rules.

- Cutting or maintenance of vegetation within lawns or landscaped areas consistent with other approvals;
- Forest management practices in response to an event-specific emergency in consultation with the Division of Forest Environment;
- Cutting within a floodplain that is located outside of all freshwater wetlands and their 100/200-foot jurisdictional areas;
- Cutting of vegetation for routine maintenance of approved landfill or remediation caps;
- Maintenance or repaving of existing parking lots with size limits;
- Addition of gates or barriers at existing or approved driveways or access roads;
- The installation of small signs associated with conservation activities;
- Crossing of or relocating an area subject to storm flowage;
- Restoration of wetlands resulting from unauthorized alterations associated with an enforcement action; and
- Establishment of a new pedestrian trail outside of wetlands and buffer zones.

Section 2.7 Protection Standards and Variance Procedures:

A new section of rules consolidated the wetland protection standards and review criteria. This section also specifies buffer protection standards.

Overall Approach to Buffer Standards: Rhode Island's freshwater wetlands resources include its rivers, streams, lakes and ponds as well as swamps, marshes, bogs, and vernal pools. About sixteen percent (16%) of Rhode Island's land area consists of freshwater wetlands (including lakes and ponds) which are distributed throughout the state. Consistent with state law and the LTF Final Report (2014), in developing the requirements, including the buffer zone widths, the agencies considered the resource characteristics, watershed protection needs and existing land uses. A framework of tiered protection was identified as the preferred approach to establishing the buffer standards. All wetlands will now be designated a buffer zone which addresses a gap in protection noted by the LTF. About 19% of vegetated wetland resources were prioritized for the greatest protection (100-foot buffer zones). This approach provides desired predictability while still allowing the buffer standards to generally account for the ecological variability associated with different wetlands. The approach allows DEM and CRMC to direct their limited resources to areas where oversight is appropriate and most needed. It further

provides the means to provide the benefit of reduced regulatory burdens on previously developed properties.

Regional Framework for Tiered Protection: Under the tiered approach, the state was divided into regions to facilitate the application of a range of buffer standards that reflect a gradient of watershed conditions. The regions consist of urban areas and non-urban areas – the latter of which is further divided into two regions to distinguish certain river buffer zones. In addition, requirements in drinking water supply watersheds were incorporated as an overlay in the non-urban regions. (See map) The tiered protective buffer zone distances were based on assessments of watershed resource protection needs, wetland resource characteristics and existing land use.

River Protection Region 1 - This Region includes watershed areas that are generally the highest priorities for conservation of fish and wildlife habitat. The region encompasses areas in the western and southeast portions of the state as well as Block Island. The watersheds exhibit low density of development, lower percentages of impervious cover and contain larger tracts of unfragmented habitat. Within this Region, in March 2019, a number of river segments in the Wood-Pawcatuck River watershed were federally designated as “wild and scenic.” River Protection Region 1 covers 52% of Rhode Island’s area (coastal waters excluded).

River Protection Region 2 - This Region includes areas of the state that exhibit a mix of land uses and watershed characteristics including urban, suburban and rural settings. Existing land use patterns have resulted in greater fragmentation of buffers within the jurisdictional areas in this region including crossings of rivers. The region includes much of the suburban development that exists adjacent to the Urban Region areas. River Protection Region 2 covers 40 % of Rhode Island’s area (coastal waters excluded).

Urban Region – This Region includes densely developed, urbanized areas of the state including portions of watersheds that contain high percentages of impervious cover and areas that are already highly developed or altered. This includes the Providence metropolitan area as well as portions of Woonsocket, Newport and West Warwick and Westerly. The Urban Region covers 8% of Rhode Island (excluding coastal waters)

Within the Regions, buffer zones were designated as follows and further described below:

- On a statewide basis, the most sensitive vegetated wetland types were identified and designated the highest level of protection (100 feet). These include unique and less common wetland types that are often sensitive to disturbance such as bogs, fens and marshes as well as evergreen swamps, swamps with Rhododendron understory and vernal pools.
- On a statewide basis, buffers zones were increased to strengthen protection in water supply reservoir watersheds.

- In River Protection Regions 1 and 2, protection was strengthened by (1) increasing the buffer zones for headwater rivers including cold water rivers as recognized as a need in the LTF Report; (2) increasing the buffer zone on larger lakes and ponds that provide high recreational and habitat value and that currently have large amounts of intact shoreline buffer; and (3) increasing the buffer zone around larger swamps some of which are also part of valuable wetland complexes providing multiple habitats.
- As practicable, buffer requirements were reduced in areas where existing land use have already resulted in the alteration or loss of vegetated buffer. This applies primarily in the Urban Region.

Section 2.23 Statewide Buffer Zone Designation

Rivers and Streams:

Current regulations designate 200 feet around rivers 10 feet or greater in width (referred to as large rivers) and 100 feet around narrower rivers and streams. The new jurisdictional area authorizes the agencies to regulate the area within 200 of rivers of any width. Within this area, buffer zones of varying widths have been designated in accordance with the following criteria.

River Protection Region 1 (Non-urban)	<ul style="list-style-type: none"> • Maintain 200 feet buffer zone on large rivers (with exceptions for a limited number of developed areas). • Increase the buffer zone from 100 to 200 feet for named rivers of high wildlife habitat value to provide stronger protection of habitat, water quality and other functions. • Increase the buffer zone to 150 feet for all other rivers. • Maintain the buffer zone of 100 feet for streams.
River Protection Region 2 (Non-urban)	<ul style="list-style-type: none"> • Maintain 200 feet buffer zone on listed large rivers with intact existing buffer. • Increase the buffer zone from 100 to 150 feet for designated cold-water rivers and other listed rivers of high wildlife habitat value. • Reduce the buffer zone from 200 to 150 feet for other listed large rivers. • Maintain the 100-foot buffer zone on smaller rivers and streams.
Urban Region	<ul style="list-style-type: none"> • Reduce the buffer zone from 200 to 150 feet along the Blackstone River and portions of the South Branch of the Pawtuxet River. • Reduce the buffer zone from 200 to 100 feet for listed large rivers. • Reduce the buffer zone from 100 to 50 feet for other rivers and streams in urbanized settings.

Drinking Water Supply Reservoirs	<ul style="list-style-type: none"> • Increase the buffer zone to 200 feet on rivers which are tributary to named public drinking water supply reservoirs in the Non-urban Region. (Not applicable outside watersheds of named reservoirs.)
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Lakes and Ponds:

Current regulations designate 50 feet around lakes and ponds greater than ¼ acre for protection. The new jurisdictional area authorizes the agencies to regulate the area within 100 feet of lakes and pond and within 200 feet of drinking water reservoirs.

River Protection Regions 1 and 2 (Non-urban)	<ul style="list-style-type: none"> • Increase buffer zone on lakes and ponds ≥10 acres from 50 to 100 feet (excluding listed lakes) • Maintain 50-foot buffer zone for lakes > ¼ acre and < 10 acres • Designate a 25-foot buffer zone on small ponds (< ¼ acre)
Urban Region	<ul style="list-style-type: none"> • Maintain the 50-foot buffer zone for listed lakes and ponds. • Designate a 25-foot buffer zone for all other ponds.
Drinking Water Supply Reservoirs	<ul style="list-style-type: none"> • Increase the buffer zone to 200 feet around named public drinking water supply reservoirs in the Non-urban Region.

Other Wetlands:

Rhode Island freshwater wetlands have been mapped in the RI Geographic Information System for non-regulatory purposes using a standardized classification scheme. The most common wetland type in Rhode Island is deciduous swamp, which is located throughout the state. Other wetland types are much less common including bog and fens. Current regulations provide a 50-foot perimeter wetland around certain wetlands of certain sizes. Small wetlands are not provided buffer protection in the current regulations. Under the draft rules all wetlands are designated with a buffer zone ranging from 25 -100 feet. Unless otherwise noted, the table below applies statewide.

Bogs and fens Marshes Evergreen forested swamps ≥ 1 acre Swamps with Rhododendron ≥ 1 acre	<ul style="list-style-type: none"> • Increase buffer zone to 100 feet with some exceptions for <i>Phragmites</i> marshes and wet meadows
Vernal Pools	<ul style="list-style-type: none"> • Increase buffer zone from 0 to 100 feet where 50% or more of the land around the pool is undeveloped vegetated land. • Increase buffer zone from 0 to 50 feet where less than 50% of the land is undeveloped vegetated land.
Shrub Swamps	<ul style="list-style-type: none"> • Increase buffer zone to 75 feet for shrub swamps ≥1 acre

	<ul style="list-style-type: none"> • Designate a 25-foot buffer zone for shrub swamps < 1 acre
Deciduous Swamps	<ul style="list-style-type: none"> • Increase buffer zone from 50 feet to 75 feet for swamps of 10 acres or more in the Non-urban Regions. • Maintain buffer zone of 50 feet for smaller swamps in the Non-urban Regions (1 to <10 acres). • Reduce the buffer zone from 50 to 25 feet around swamps in Urban Region. • Designate a 25- foot buffer zone for swamps < 1 acre.

Naturally vegetated, undeveloped areas within buffer zones will be expected to be maintained as buffer.

Application Types and Permitting Process: The draft rules make changes to the existing permitting processes to improve clarity and predictability and reduce regulatory burdens.

- The DEM Preliminary Determination permit outlined in the current rules will be replaced with a “Freshwater Wetland Permit” (§ 2.11).
- Applicants that meet all standards (§ 2.7.1), including the buffer standard, will have reduced submittal requirements from the current Preliminary Determination Application. The draft rules include a new variance procedure for those situations in which the standard cannot be met. Variance requirements are outlined in § 2.7.3.
- The draft rules include an in-fill lot standard that acknowledges constraints on certain existing lots of record and allows processing of a permit without a variance (§ 2.7.1(B)(5)).
- The draft rules also establish a process for DEM to issue a “General Permit” for certain categories of projects with predictable and limited impacts. The issuance of general permits would occur at a future date and involve specifying the requirements and conditions under which such a permit would apply. This is intended to have reduced submittal requirements and review times in comparison to an Application for a Freshwater Wetlands Permit (§ 2.11).
- The draft rules provide new and clarify or expand certain exemptions for limited activities that do not present impacts to freshwater wetlands that merit review. See § 2.6.
- Simplified the application fee schedule. See § 2.8.9.

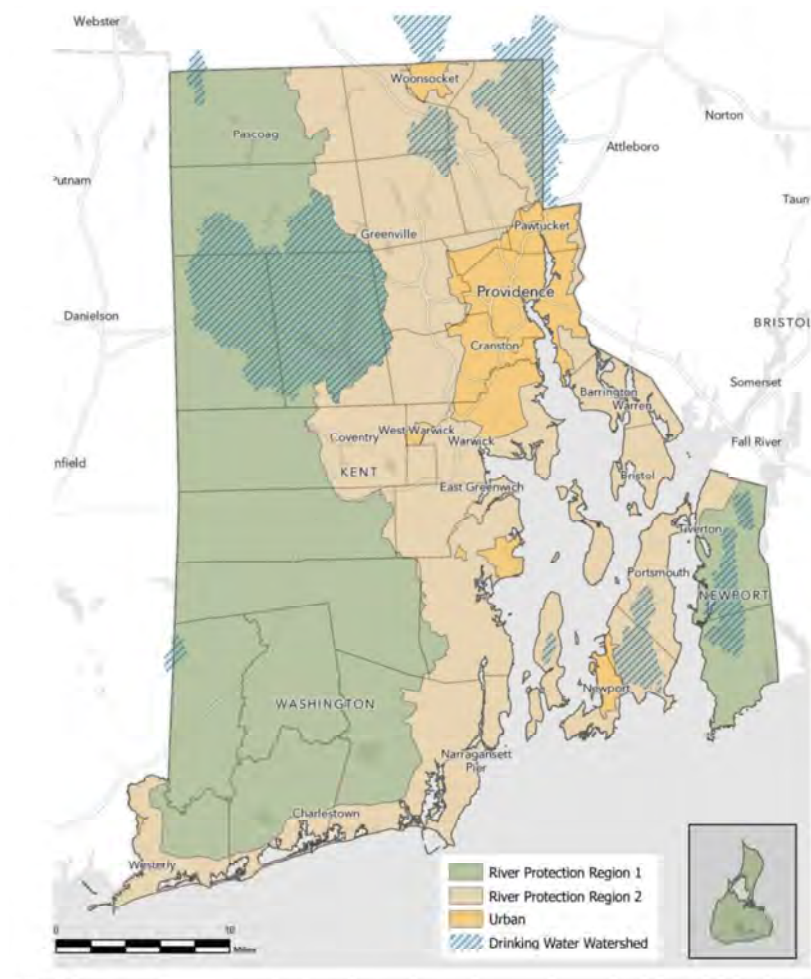
Municipal Coordination (§ 2.8.11): The draft rules have provisions to strengthen coordination with municipalities including the following:

- Notification of designated municipal officials of when permits are filed with state agencies;

- Require applicants for major land development projects to obtain master plan approval, pursuant to R.I. Gen. Laws § 45-23-40, prior to filing for a state freshwater wetland permit. Applicants are encouraged to obtain verified wetland edges as part of project planning.
- Opportunity to provide local input while maintaining timeliness within the state permitting programs;
- § 2.16 - Procedure for the municipality to petition the DEM or CRMC to increase the size of a buffer zone for a particular wetland resource (the requested buffer zone cannot exceed the jurisdictional area).

Agricultural Activities: For clarity the rules pertaining to agricultural activities have been grouped into its own section 2.13. Note that per state law, the regulatory oversight of normal farming and certain related activities for farmers as qualified under the law has not changed; e.g. the expanded jurisdictional area does not apply.

Figure 1: Freshwater wetlands buffer regions map (2020)



APPENDIX H

Opportunities and Challenges Map Set

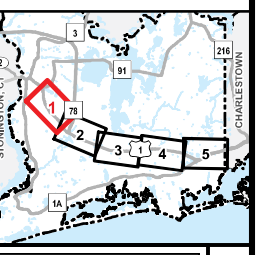
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
Opportunities & Challenges

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

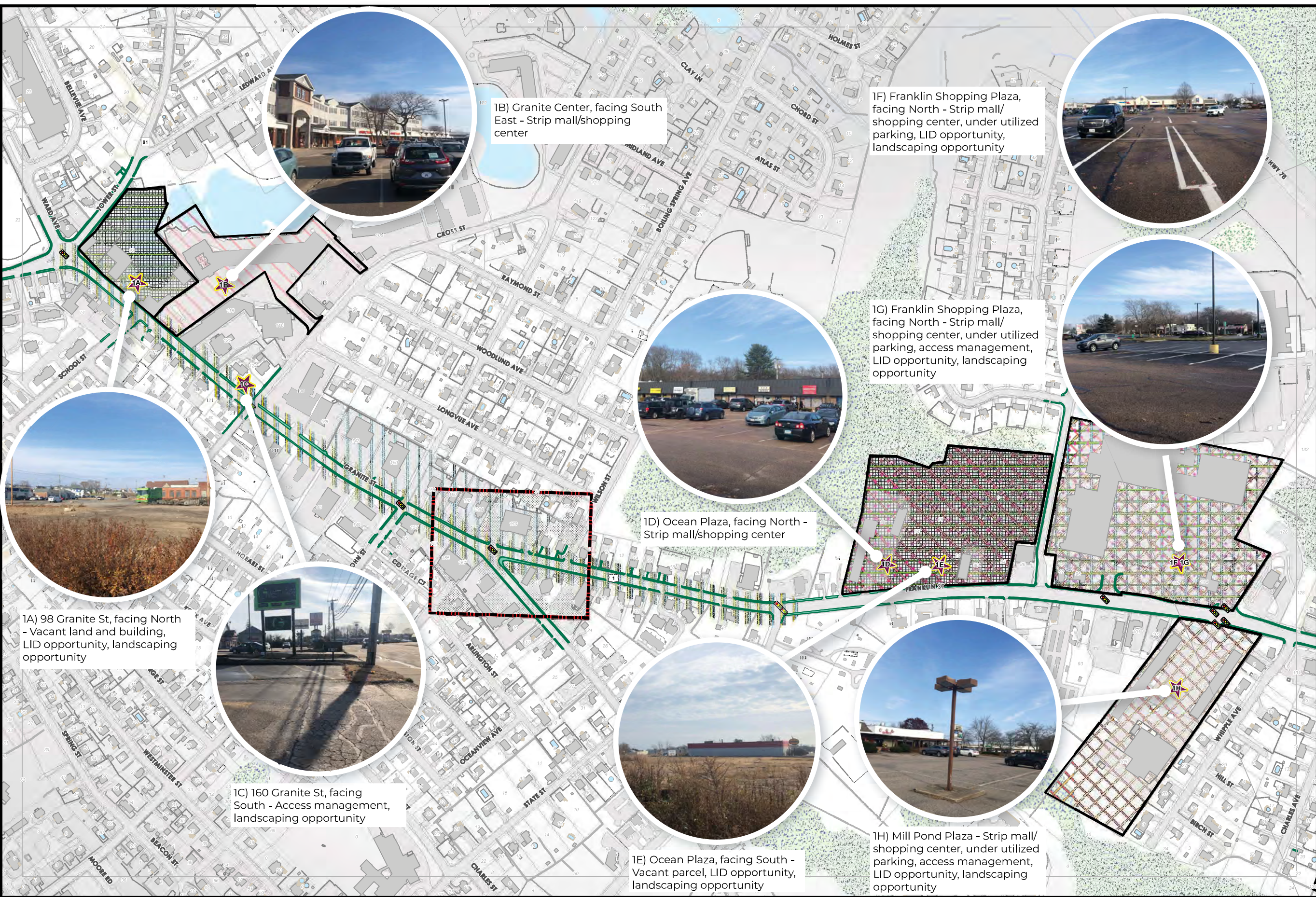
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- Legend**
- Traffic Signal
 - Fence
 - Retaining Wall
 - Rock Wall
 - Wall
 - Building
 - Deck
 - Swimming Pool
 - Streams
 - Lakes & Ponds
- Opportunity and Challenges Areas**
- Access management areas
 - Pedestrian access (sidewalks)
 - Landscape improvement area
 - Underutilized parking areas
 - Intersections
 - Strip malls / shopping plazas
 - Vacant buildings/parcels
 - Low impact development area
 - Motel/hotel
 - Paved Sidewalks Within Corridor
 - Photo Point (see appendix)



OC-1

MAP SHEET 1 of 5



1B) Granite Center, facing South East - Strip mall/shopping center

1F) Franklin Shopping Plaza, facing North - Strip mall/shopping center, under utilized parking, LID opportunity, landscaping opportunity

1G) Franklin Shopping Plaza, facing North - Strip mall/shopping center, under utilized parking, access management, LID opportunity, landscaping opportunity

1D) Ocean Plaza, facing North - Strip mall/shopping center

1A) 98 Granite St, facing North - Vacant land and building, LID opportunity, landscaping opportunity

1C) 160 Granite St, facing South - Access management, landscaping opportunity

1E) Ocean Plaza, facing South - Vacant parcel, LID opportunity, landscaping opportunity

1H) Mill Pond Plaza - Strip mall/shopping center, under utilized parking, access management, LID opportunity, landscaping opportunity

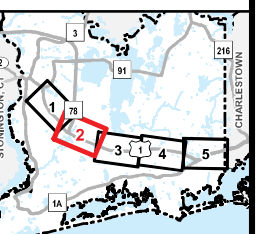


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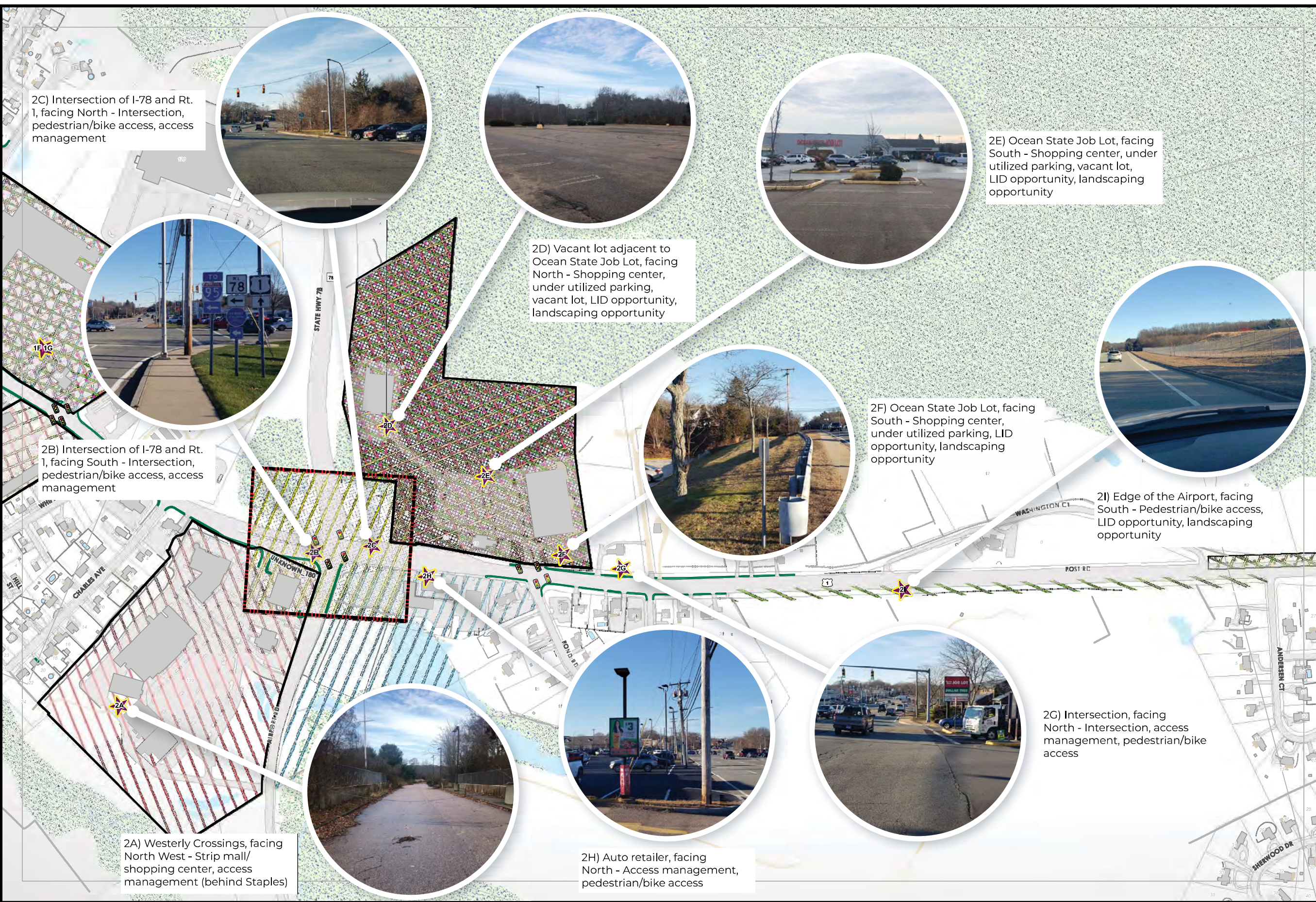
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Legend

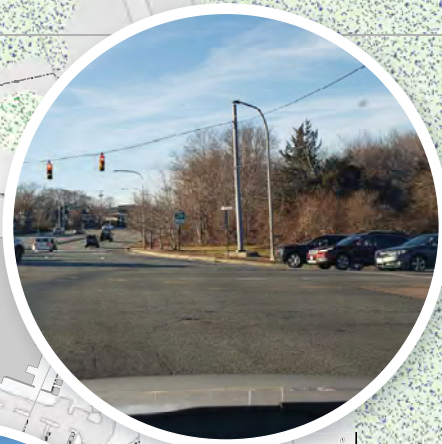
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 - Intersections
 - Strip malls / shopping plazas
 - Vacant buildings/parcels
 - Low impact development area
 - Motel/hotel
 - Paved Sidewalks Within Corridor
 - Photo Point (see appendix)



OC-2



2C) Intersection of I-78 and Rt. 1, facing North - Intersection, pedestrian/bike access, access management



2D) Vacant lot adjacent to Ocean State Job Lot, facing North - Shopping center, under utilized parking, vacant lot, LID opportunity, landscaping opportunity

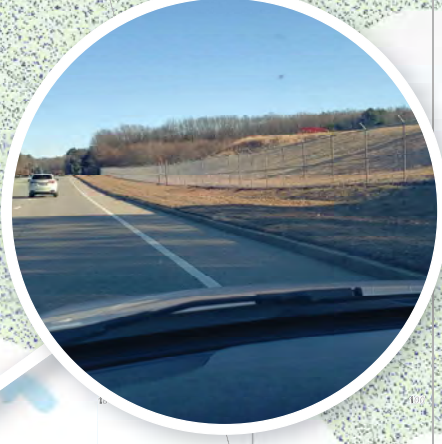


2E) Ocean State Job Lot, facing South - Shopping center, under utilized parking, vacant lot, LID opportunity, landscaping opportunity

2B) Intersection of I-78 and Rt. 1, facing South - Intersection, pedestrian/bike access, access management



2F) Ocean State Job Lot, facing South - Shopping center, under utilized parking, LID opportunity, landscaping opportunity



2I) Edge of the Airport, facing South - Pedestrian/bike access, LID opportunity, landscaping opportunity

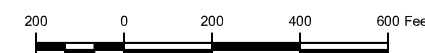
2A) Westerly Crossings, facing North West - Strip mall/shopping center, access management (behind Staples)



2H) Auto retailer, facing North - Access management, pedestrian/bike access



2G) Intersection, facing North - Intersection, access management, pedestrian/bike access



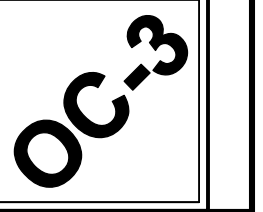
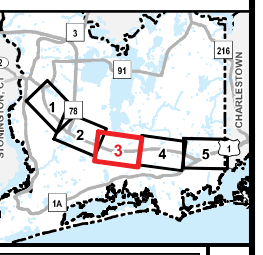
WESTERLY, RHODE ISLAND
ROUTE 1 CORRIDOR
Opportunities & Challenges

DECEMBER 2021

Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly/Rhode Island.

Disclaimer:
 This information is for planning purposes only. It should not be considered exact. Field inspection and verification from schematic maps.

- Legend**
- Traffic Signal
 - Fence
 - Retaining Wall
 - Rock Wall
 - Wall
 - Building
 - Deck
 - Swimming Pool
 - Streams
 - Lakes & Ponds
- Opportunity and Challenges Areas**
- Access management areas
 - Pedestrian access (sidewalks)
 - Landscape improvement area
 - Underutilized parking areas
 - Intersections
 - Strip malls / shopping plazas
 - Vacant buildings/parcels
 - Low impact development area
 - Motel/hotel
 - Paved Sidewalks Within Corridor
 - Photo Point (see appendix)



MAP SHEET 3 of 5



3A) Aquastar Inn, facing North - Hotel/motel, access management, landscaping opportunity, LID opportunity, pedestrian/bike access



3C) Intersection of Rt. 1 and Memory Lane, facing South - Intersection, pedestrian/bike access, access management [source: Google street view]



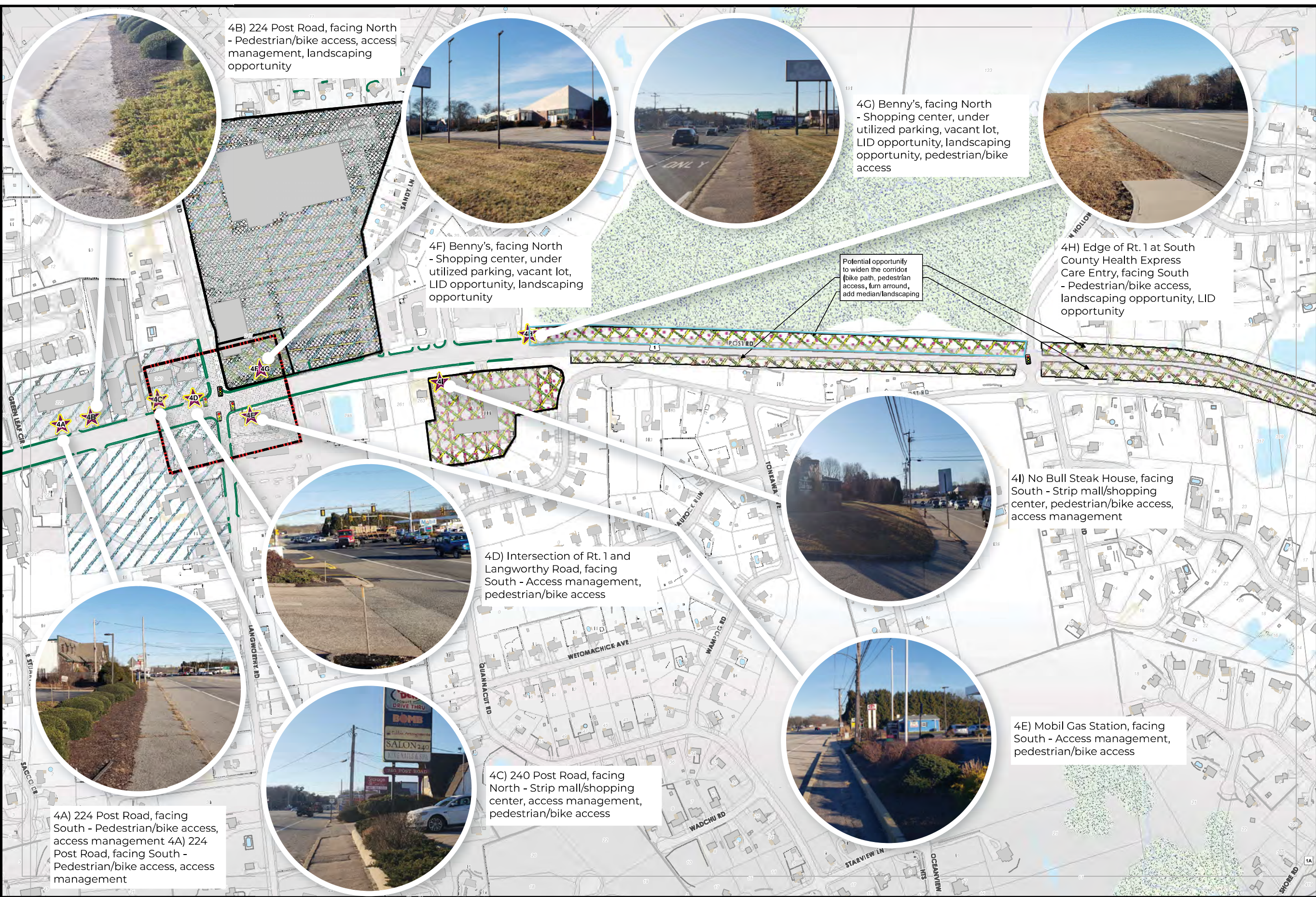
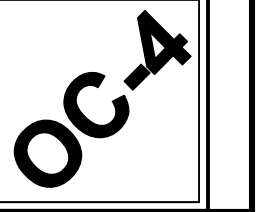
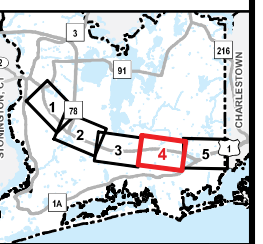
3B) Intersection of Rt. 1 and Old Post Road, facing South - Intersection, pedestrian/bike access [source: Google street view]



Data Sources:
 Data from Rhode Island Geographic Information System (RIGIS) and Town of Westerly Rhode Island.

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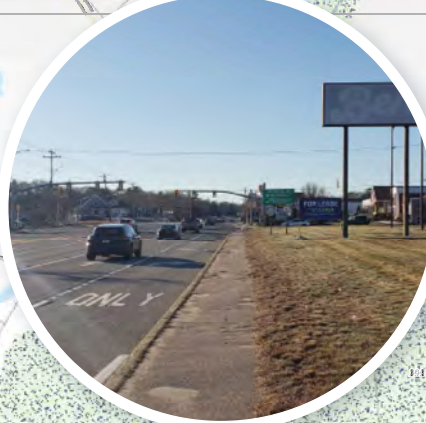
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4B) 224 Post Road, facing North - Pedestrian/bike access, access management, landscaping opportunity



4F) Benny's, facing North - Shopping center, under utilized parking, vacant lot, LID opportunity, landscaping opportunity



4G) Benny's, facing North - Shopping center, under utilized parking, vacant lot, LID opportunity, landscaping opportunity, pedestrian/bike access



4H) Edge of Rt. 1 at South County Health Express Care Entry, facing South - Pedestrian/bike access, landscaping opportunity, LID opportunity

Potential opportunity to widen the corridor (bike path, pedestrian access, turn around, add median/landscaping)

4D) Intersection of Rt. 1 and Langworthy Road, facing South - Access management, pedestrian/bike access



4I) No Bull Steak House, facing South - Strip mall/shopping center, pedestrian/bike access, access management



4A) 224 Post Road, facing South - Pedestrian/bike access, access management
 4A) 224 Post Road, facing South - Pedestrian/bike access, access management



4C) 240 Post Road, facing North - Strip mall/shopping center, access management, pedestrian/bike access



4E) Mobil Gas Station, facing South - Access management, pedestrian/bike access



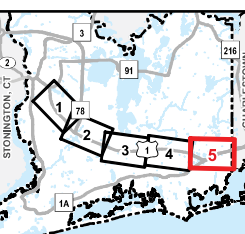
WESTERLY, RHODE ISLAND
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OC-5

MAP SHEET 5 of 5



5A) Intersection of Rt. 1 and Shore Road, facing North - Pedestrian/bike access, landscaping opportunity, LID opportunity [source: Google street view]



5B) Intersection of Rt. 1 and Warren Road, facing South - Pedestrian/bike access, LID opportunity, landscaping opportunity

